



## **Town of Arlington, MA Redevelopment Board**

### **Agenda & Meeting Notice July 22, 2019**

The Arlington Redevelopment Board will meet Monday, July 22, 2019 at 7:30 PM in the **Charles Lyons Hearing Room, Town Hall, 2nd Floor, 730 Mass. Ave., Arlington, MA 02474**

#### **1. Environmental Design Review, 1207-1211 Massachusetts Avenue, Arlington, MA - Docket #3602 Public Hearing**

7:30 p.m. -  
8:30 p.m.

Board will open Special Permit Docket #3602 to review application by James F. Doherty for 1211 Mass Ave Realty Trust, at 1207-1211 Massachusetts Avenue, Arlington, MA, 02476, to construct a 50 room hotel and restaurant at 1207-1211 Massachusetts Avenue within the B2 Neighborhood District and the B4 Vehicular Oriented Business District in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4 Environmental Design Review.

- Applicant will be provided 10 minutes for an introductory presentation.
- DPCD staff will be provided 5 minutes for an overview of their Public Hearing Memorandum.
- Members of the public will be provided time to comment.
- Board members will discuss docket and vote.

#### **2. ARB Meeting Minutes**

8:30 p.m. -  
8:40 p.m.

- Board members will review and approve meeting minutes

#### **3. Open Forum**

8:40 p.m. -  
9:00 p.m.

- Except in unusual circumstances, any matter presented for consideration of the Board shall neither be acted upon, nor a decision made the night of the presentation. There is a three minute time limit to present a concern or request.

#### **4. Adjourn**

9:00 p.m. - Adjourn

#### **5. Correspondence received:**

Letter from Steve Revilak 6/27/2019  
LeRoyer Comments on Proposed Hotel



## Town of Arlington, Massachusetts

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#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	Agenda_Item_1_-_1207-1211_Mass_Ave_Application_Materials.pdf	1207-1211 Mass Ave Application Materials
▢ Reference Material	Agenda_Item_1_-_1207-1211_Mass_Ave_Plans.pdf	1207-1211 Mass Ave Plans
▢ Reference Material	Agenda_Item_1_-_EDR_Public_Hearing_Memo_Docket_#3602_1207-1211_Mass_Ave_Final.pdf	EDR Public Hearing Memo Docket #3602 1207-1211 Mass Ave
▢ Reference Material	Agenda_Item_1_-_LEED_v4_for_Building_Design_and_Construction_1_PAGE_071819.pdf	LEED v4 for Building Design and Construction
▢ Reference Material	Agenda_Item_1_-_Memo_from_J.Doherty_07-18-19.pdf	Memo from J. Doherty 07-18-19



TOWN OF ARLINGTON  
REDEVELOPMENT BOARD

Application for Special Permit In Accordance with Environmental Design  
Review Procedures (Section 3.4 of the Zoning Bylaw)

Docket No. \_\_\_\_\_

1. Property Address 1207 & 1211 Massachusetts Ave  
Name of Record Owner(s) 1211 MASS AVE Realty Trust\* Phone 781-640-2942  
Address of Owner 1122 MASS AV, ARLINGTON, MA 02476  
Street City, State, Zip

2. Name of Applicant(s) (if different than above) N/A Phone \_\_\_\_\_  
Address \_\_\_\_\_  
Status Relative to Property (occupant, purchaser, etc.) \_\_\_\_\_

3. Location of Property Parcels 58-11-1 & 57-4-14  
Assessor's Block Plan, Block, Lot No.

4. Deed recorded in the Registry of deeds, Book 5873, Page 485; 60543 439  
-or- registered in Land Registration Office, Cert. No. \_\_\_\_\_, in Book \_\_\_\_\_, Page \_\_\_\_\_.

5. Present Use of Property (include # of dwelling units, if any) residential, Automotive,  
VACANT SOCIAL CLUB

6. Proposed Use of Property (include # of dwelling units, if any) MIXED-USE CONSISTING  
OF A FIFTY ROOM HOTEL AND RESTAURANT ON THE FIRST  
FLOOR.

7. Permit applied for in accordance with the following Zoning Bylaw section(s) S.5.3 Mixed use development  
S.3.17 Upper story setbacks  
6.1.5 Parking reduction in Business district  
S.3.6 Exceptions to maximum FAR regulation  
section(s) title(s)

8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

\*TOWN OF ARLINGTON

SEE ATTACHED

(In the statement below, strike out the words that do not apply)

The applicant states that 1211 MASS AVE Realty Trust is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 1211 & 1207 Massachusetts Ave.

which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

[Signature] Trustee  
Signature of Applicant(s)

1122 Massachusetts Ave ARLINGTON, MA. 781-640-2942  
Address Phone



Town of Arlington Redevelopment Board  
Application for Special Permit in accordance with  
Environmental Design Review (Section 3.4)

Required Submittals Checklist

Two full sets of materials and one electronic copy are required. A model may be requested. Review the ARB's Rules and Regulations, which can be found at [arlingtonma.gov/arb](http://arlingtonma.gov/arb), for the full list of required submittals.

- ☒ Dimensional and Parking Information Form (see attached)
- ☒ Site plan of proposal
- ☐ Model, if required
- ☒ Drawing of existing conditions - photo
- ☒ Drawing of proposed structure
- ☒ Proposed landscaping. May be incorporated into site plan
- ☒ Photographs
- ☒ Impact statement
- ☒ Application and plans for sign permits
- ☐ Stormwater management plan (for stormwater management during construction for projects with new construction ~)

FOR OFFICE USE ONLY

- |                                                                                                    |                      |
|----------------------------------------------------------------------------------------------------|----------------------|
| <input type="checkbox"/> Special Permit Granted                                                    | Date: _____          |
| <input type="checkbox"/> Received evidence of filing with Registry of Deeds                        | Date: _____          |
| <u>62</u> <input checked="" type="checkbox"/> Notified Building Inspector of Special Permit filing | Date: <u>6/24/19</u> |



# TOWN OF ARLINGTON

Dimensional and Parking Information  
for Application to  
The Arlington Redevelopment Board

Docket No. \_\_\_\_\_

Property Location 1207 + 1211 MASSACHUSETTS AVE

Zoning District B4/B2

Owner: 1211 MASS AVE REALTY TRUST

Address: 1211 MASSACHUSETTS AV

Present Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

Mixed use / residential, automotive, social club

5,731 SF

Proposed Use/Occupancy: No. of Dwelling Units:

Uses and their gross square feet:

Mixed use / Hotel, Restaurant

24,443 SF

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	14,030	14,030	min.
Frontage	160.12	160.12*	min. 50
Floor Area Ratio	.41	1.67	max. 1.8
Lot Coverage (%), where applicable	—	—	max. N/A
Lot Area per Dwelling Unit (square feet)	—	—	min. —
Front Yard Depth (feet)	10	4.7	min. —
Side Yard Width (feet) right side	—	—	min. —
left side	—	—	min. —
Rear Yard Depth (feet)	16	38	min. 18
Height			min. —
Stories	2	4	stories 4-5
Feet	25	44	feet 50'
Open Space (% of G.F.A.)			min. —
Landscaped (square feet)	1170	1581	(s.f.)
Usable (square feet)	1670	3384	(s.f.)
Parking Spaces (No.)	24	28	min. 50
Parking Area Setbacks (feet), where applicable	0	5'	min. 5'
Loading Spaces (No.)	0	0	min. —
Type of Construction	Type 2B + 5		
Distance to Nearest Building	0	.06	min. 0

\* MASSACHUSETTS AVENUE frontage only

Hand Delivered

2019 JUN 21 A 11:15

June 20, 2019

Redevelopment Board

Town of Arlington

730 Massachusetts Avenue

Arlington, Ma. 02476

**RE: Special Permit Application – 1207 & 1211 Massachusetts Avenue**

Dear Members:

Enclosed please find our application for Special Permit in accordance with Environmental Design Review Procedures (Section 3.4 of the Arlington Zoning Bylaw). Included in this application are the submittals outlined on the check list. We have spent many hours developing these plans with input from the Planning department staff, which we are very appreciative of. These discussions have been very helpful and we look forward to discussing this proposal with you.

The proposed development would demolish the current improvements on both parcels and construct a four story mixed use development. When complete, the property will consist of a 50 room boutique hotel and upscale restaurant. This proposal will provide much needed improvements to the area and a significant economic stimulus to the Heights. Below we address the narrative relating to the Impact Statement and Special Permit Criteria.

**Impact Statement:**

***1. Preservation of Landscaping.***

The current site has a small amount of landscaping in front of the former DAV Post. The balance of the site is covered by the buildings, pavement, and walkways on the site. We intend to provide a larger area with a substantial addition of trees, plants, and landscaping along the rear of the site, providing a nice lush buffer to the residential district to the rear. In addition we have an extensive planting and open space design for the front of the site along Massachusetts Avenue. Please see the attached site plan and planting schedule.

***2. Relation of Buildings to Environment.***

As mentioned above, this proposal involves a four story elevator building consisting of a hotel and restaurant. The hotel lobby and restaurant will be located on the first floor while all guest rooms will be on the upper levels. The fourth floor will also include private deck space for each unit on that level, as well as a grassed area for other hotel guests. The proposal will move the structure closer to Massachusetts Avenue and much further away for the residential neighborhood to the rear. This project is in harmony with the retail and other uses in the area.

### ***3. Open Space.***

The site is currently improved with two structures having a combined footprint of 4,614 sq. ft. The proposed structure will have a footprint of 5,516 sq. ft., an increase of 902 sq. ft. Although there is a slight increase, the usable open space, substantial pervious area reduction and rear yard setbacks are all positive results of this project.

### ***4. Circulation.***

The improvements proposed will help the circulation for vehicular, bicycle, and pedestrians. We will be eliminating two large driveway openings (totaling 55') and realigning the sidewalk and curbing. In addition we are providing indoor and outdoor areas for bicycles. All parking will be provided via a single curb cut in the rear. Bus service to multiple locations can be boarded / dropped off within feet of the property and is convenient to highway access and the bike path.

### ***5. Surface Water Drainage.***

The properties are currently covered (over 90%) by impervious surfaces. The proposal will result in a reduction in impervious surface, therefore Title 5, article 15, section 4 does not apply. However, we have met with the Assistant Town Engineer and have agreed to construct a storm water management system onsite. The system will be located under the driveway and contain all roof and surface runoff. All surface water will be contained on site, in compliance to the bylaws and with Town approval.

### ***6. Utility Service.***

As part of the redevelopment, all new utility services will be installed to the property. These systems will all be underground and conform to Town requirements.

### ***7. Advertising Features.***

As shown on the renderings, we are proposing signage for the hotel and restaurant. It will appear on the front and West side. In Addition there will be some small signs (most likely two or three) in the rear to guide vehicular, bicycle and pedestrian traffic. On the front the signage will be a contemporary font (12") and mounted to the front of the canopy to the hotel and restaurant. The signage on Clark Street will be on the building façade, the fonts ( Hotel sign 12", restaurant 8") will match the canopy signage. Both the front canopy and the Clark Street signage will be back lit.

## **8. *Special Features.***

We have proposed substantial landscaping on site, specifically the rear boundary. This is intended to provide adequate screening and create a more harmonious environment than currently exist.

## **9. *Safety.***

We believe the proposed improvements to the sidewalks and the elimination of two driveway openings will create safer off site conditions for residents. In addition, the building has two stairwells servicing all floors as well as an elevator. It will meet all ADA and fire code requirements. Illumination of the rear parking area will be achieved by 12'-0" high pole mounted LED lights with cut off lenses to ensure no other properties are affected. Additional LED down lights will be mounted below the projecting balcony to illuminate the area at the building covered parking entrance.

## **10. *Heritage.***

This project does not involve any historical structures, nor will it disrupt any historical uses. In fact we believe that this project will increase interest in the Towns many Historic sites. Located a short distance from the property is the "FOOT OF THE HILLS" site and "OLD SCHWAMB MILL." It is our goal to leverage the Lexington tour groups, and introduce them to the Town where the first shot of the Revolution was fired!

## **11. *Microclimate.***

We believe the increase in permeable surface will impact light, air, and water recharge in a positive way. In addition this will also create a nice natural buffer. The new structure will be 35 to 54 feet away from the rear boundary, which is a significant increase from the current conditions which ranges from 16 to 20 feet.

The basement level has a sizable mechanical equipment room serving the main street level public spaces (the Hotel Lobby and the Restaurant). All of the upper floor hotel rooms are served by individual vertical air handlers (V-TAC) units, as depicted by the louvers on the building elevations and renderings. All of the equipment is designed and located to control any emissions. The entire building will be exhausted through the high roof with low profile exhaust fans.

## **12. *Sustainable Building and Site Design.***

This building will meet or exceed the Towns New Stretch Code. Below we have provided some details of the exterior finishes being proposed.

**Main Level:** Kawneer Curtain wall system, making the public spaces as transparent (inviting) as possible;

2<sup>ND</sup> & 3<sup>RD</sup> Floor: The cantilevered (projected) bays consist of an insulated stucco system, the recess portions consist of either actual 1"x finished wood or a "Nichia" cladding having the appearance of wood.

4<sup>TH</sup> Floor: Is clad with the insulated stucco system.

Both the high and low roofs are copped with a darker metal roof edge system.

**Clark Street Façade:**

Main Level: (see above for the public spaces); The stair well is clad with a metal panel system similar to Corten or equal.

2<sup>ND</sup> & 3<sup>RD</sup> Floor: Consists of either actual 1"x finished wood or a "Nichia" cladding having the appearance of wood. The stair well is clad with a metal panel system similar to Corten or equal.

4<sup>TH</sup> Floor: Is clad with the insulated stucco system. The stair well is clad with a metal panel system similar to Corten or equal.

Both the high and low roofs are copped with a darker metal roof edge system.

All windows occurring in the stair well are clad with a metal louver system.

**Rear Façade:**

Lower Level: The main field is clad with either actual 1"x finished wood or a "Nichia" cladding having the appearance of wood. Both ends of the building are clad with a metal panel system similar to Corten or equal.

Main Level: The main field is clad with either actual 1"x finished wood or a "Nichia" cladding having the appearance of wood. Both ends of the building are clad with a metal panel system similar to Corten or equal.

2<sup>ND</sup> & 3<sup>RD</sup> Floor: The cantilevered (projected) bays consist of an insulated stucco system, the recess portions consist of either actual 1"x finished wood or a "Nichia" cladding having the appearance of wood.

4<sup>TH</sup> Floor: Is clad with the insulated stucco system. The stair well is clad with a metal panel system similar to Corten or equal. Both ends of the building are clad with a metal panel system similar to Corten or equal.

Both the high and low roofs are copped with a darker metal roof edge system.

**Right Side Façade:**

A portion of the lower level is a common wall. The remainder of the exterior cover will be similar to the design of the rear façade.

**Special Permit Criteria**

- This mixed use project is proposed in the B4/B2 zoning districts. Mixed use is an allowable use, provided a Special Permit is issued by the Board. This proposal was submitted as a response to an RFP issued by the Town of Arlington for a mixed use development at 1207 Massachusetts Avenue. We seek approval of a special permit from the "Use Regulations For Business Districts" section 5.5.3.
- The proposal calls for a four story building containing a hotel and restaurant. The pertinent section of the bylaw, Section 5.3.17, provides for any building over three stories in height to have a "step in" of 7.5' at the third floor level or 30'.

The proposed design “steps in” the first floor 8” from the beginning of the second and third floors and again at the top of the third floor. The result of this design has the main façade comprised of only two floors (where as the bylaw allows for three), with a “step in” at 34’ rather than 30’.

We believe that this design not only meets the intent of the bylaw, but provides even greater reduction in massing. We therefore request the Board provide relief.

- The proposal increases the amount of parking on the site partially by providing 20 spaces under the building in addition to 8 outside spaces. The bylaw calls for 50 spaces and therefore we are seeking relief in accordance with section 6.1.5 of the bylaw (Please see attached parking summary grid).

The proposal provides substantial indoor and outdoor parking for bicycles on a long term and short term basis. Both areas are easily accessible and not only provide for bicycle storage, but there will be a bench and some tools available. In addition we are providing maps of the local bike network and other information for bicyclists (please see attached bicycle storage summary).

As required in the bylaw we have also included a Traffic Reduction Plan which provides many more details on our plan to comply with the bylaw. Below I have outlined a few additional commitments we are proposing;

Charge for parking off site;

Provide preferential parking for carpool vehicles;

Provide transit pass subsidies;

Provide covered bicycle parking and storage.

- The proposal is located in the zoning districts of B4 & B2, which has an FAR of 1.2. According to section 5.3.6 “Increase in Maximum Floor Area Ratio” the Board may provide relief for mixed use building area in these districts.

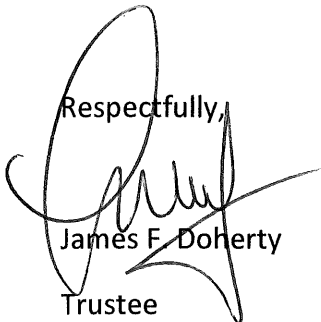
As noted, we are substantially increasing the permeable and open space on the site. This will provide for a much larger buffer to the residential district to the rear. The design of the building and landscaping plan will also provide landscaping and open space along the entire Massachusetts Avenue frontage. A portion of this area also will provide a nice open area for local community performances and art shows or presentations regarding the local historical locations nearby. We will also be dedicating an area in the lobby of the hotel to a local Veteran and community person.

Our request for an increase of 2,398 sf of building area, equates to an 11% increase. We believe we meet the criteria for relief.

1. The proposed development will transform a blighted area and provide a missing dining option in the Heights as well as a Hotel for visitors and relatives of residents. Town Meeting has changed the zoning to encourage mixed use development for the convenience and welfare of the public.
2. The property currently has three very large curb cuts on Massachusetts Avenue and Clark Street. As part of this proposal we will be removing the two on Massachusetts Avenue and shortening the Clark Street opening. We believe this and other adjustments to the sidewalk in front will improve both pedestrian safety and traffic congestion.
3. This project will not overload any public water, drainage or sewer system. We are proposing to reduce the impervious surface and install a storm water management system on site. There currently are none.
4. The current improvements consist of automotive repair and sales, as well as a former social club. The proposed development will complement the new leader bank and hopefully stimulate of retail activity in the property at 1215 Massachusetts Avenue. This proposal will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals or welfare. Our goal is to work with the Redevelopment Board and staff to ensure that any special regulations for this use, as provided in the Bylaw, are fulfilled.
5. The only other hotel in Arlington is on the Cambridge line. Not only will this proposed use not cause an excess of this use, we believe this will provide economic stimulus to the retail district in the Heights, draw visitors from Lexington to the historic sites of Arlington and provide an upscale dining and gathering area for the neighborhood.

We believe this application is in the spirit of what Town Meeting envisioned when it adopted the mixed use section to the Zoning Bylaw. The RFP waived all permitting fees associated with this development, therefore no application fee was included. We look forward to meeting with the Redevelopment Board. As a result of this process, we request the Board approve our Petition for Environmental Design Review and Special Permits requested.

Respectfully,



James F. Doherty  
Trustee

**1211 Massachusetts Avenue**  
**Traffic Demand Management Plan (TDM)**

This property is located in one of the most ideal locations to support a parking reduction request. The site is located within 50 yards of an East / West bound MBTA surface bus stop. In addition there is a central terminal and other bus lines within walking distance.

The site is in close proximity to the Minuteman Bike Path (approximately 600 yards) which can be accessed by a street directly adjacent to the property. In addition there are marked bike lanes on Massachusetts Avenue. Both of these options provide a practical and safe route for guests. To encourage this use we will be providing an outdoor and indoor location for bicycle storage.

To help promote ridesharing we will be working with local taxi operators, livery services, and shuttles which connect areas like Alewife to employment hubs on Route 128. We also are in discussions with ZIPCAR for a spot at the site.

Finally we are discussing other nearby locations to obtain Valet parking, if necessary. It is our belief that this plan addresses the Bylaw and as mentioned previously, is a deserving location to warrant the requested relief.



<b><i>Parking Summary</i></b>			
<b><i>1211 Massachusetts Ave</i></b>			
<b><u>Spaces Required:</u></b>			
<b>Use</b>	<b>Quantity</b>	<b># of spaces</b>	<b>Total Required</b>
Hotel	50	50	50
Resturant	2,568 sf	0	0
Total			50
Proposed			28
Reduction			22

<b><i>Bicycle Storage Summary</i></b>			
<b><i>1211 Massachusetts Ave</i></b>			
<b><u>Spaces Required:</u></b>			
<b><u>Use</u></b>	<b><u>Short Term</u></b>	<b><u>Long Term</u></b>	<b><u>Total Required</u></b>
Hotel	3	1	4
Resturant	2	1	3
<b>Total</b>	<b>5</b>	<b>2</b>	<b>7</b>
<b>Proposed</b>	<b>7</b>	<b>7</b>	<b>14</b>

<b><i>Planting Schedule</i></b>				
<b><i>1211 Massachusetts Ave</i></b>				
<b><u>Quantity</u></b>	<b><u>Botanical Name</u></b>	<b><u>Common Name</u></b>	<b><u>Notes</u></b>	<b><u>Location</u></b>
75	Buxus Green Velvet	Green Velvet Boxwood	15-18"	Front
75	Carex Blue Zinger	Blue Zinger Grass	n/a	Front
4	Syringa Reticulata Ivory Silk	Tree Liliac	2"	Rear
10	Thujastandishi x Pucata	Green Grant Arborvitae	6-8'	Rear
4	Ilex x Meserveae	Blue Princess	8-10"	Rear
4	Hydrangea Quercifolia	Oakleaf Hydrangea	4'	side

# SPECIAL PERMIT - SITE PLAN REVIEW

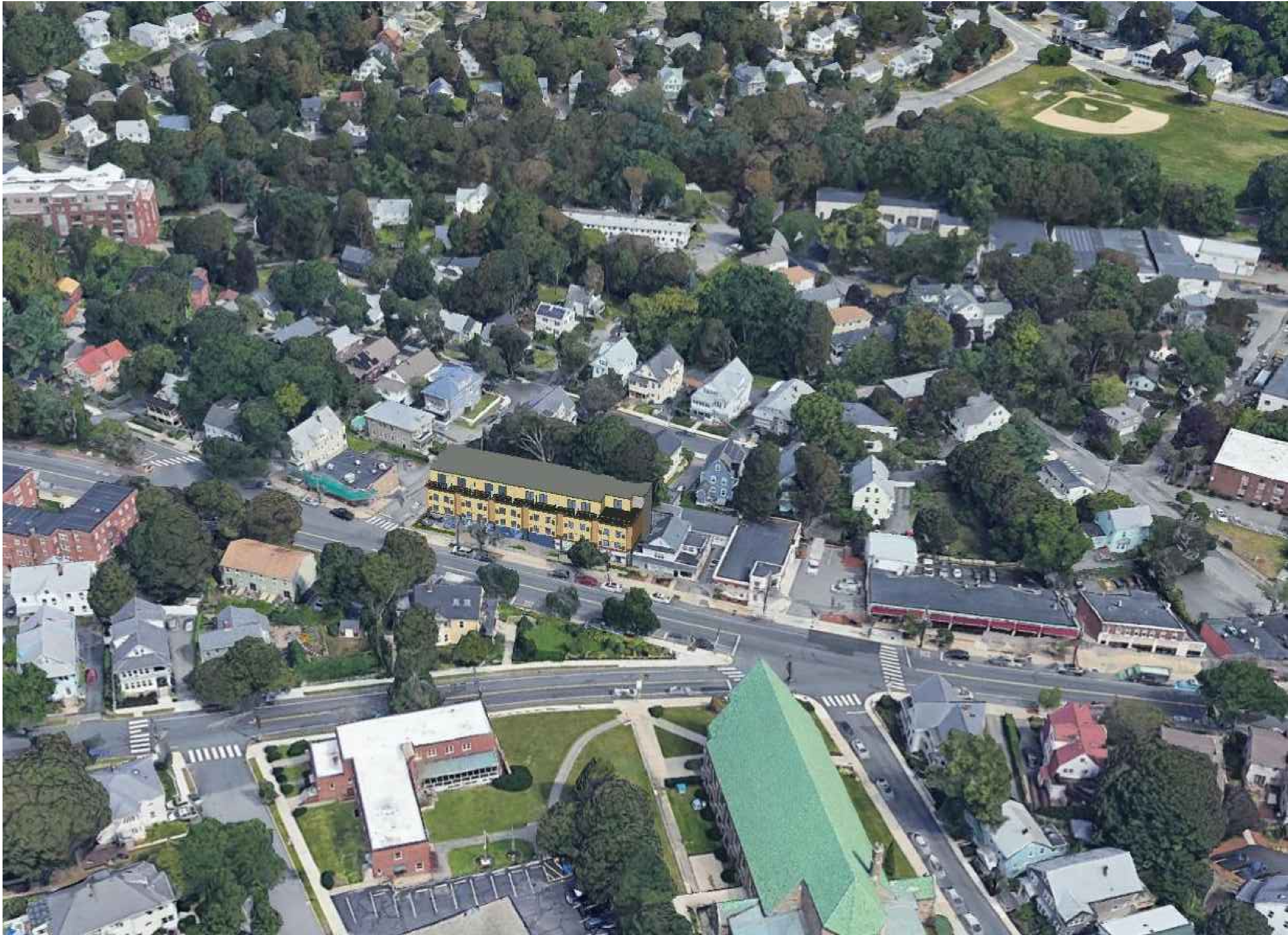
1211 Massachusetts Avenue  
Arlington, MA 02476

June 20, 2019



LINCOLN ARCHITECTS LLC  
1 Mount Vernon Street, Suite 203  
Winchester, MA 01890  
781.721.7721

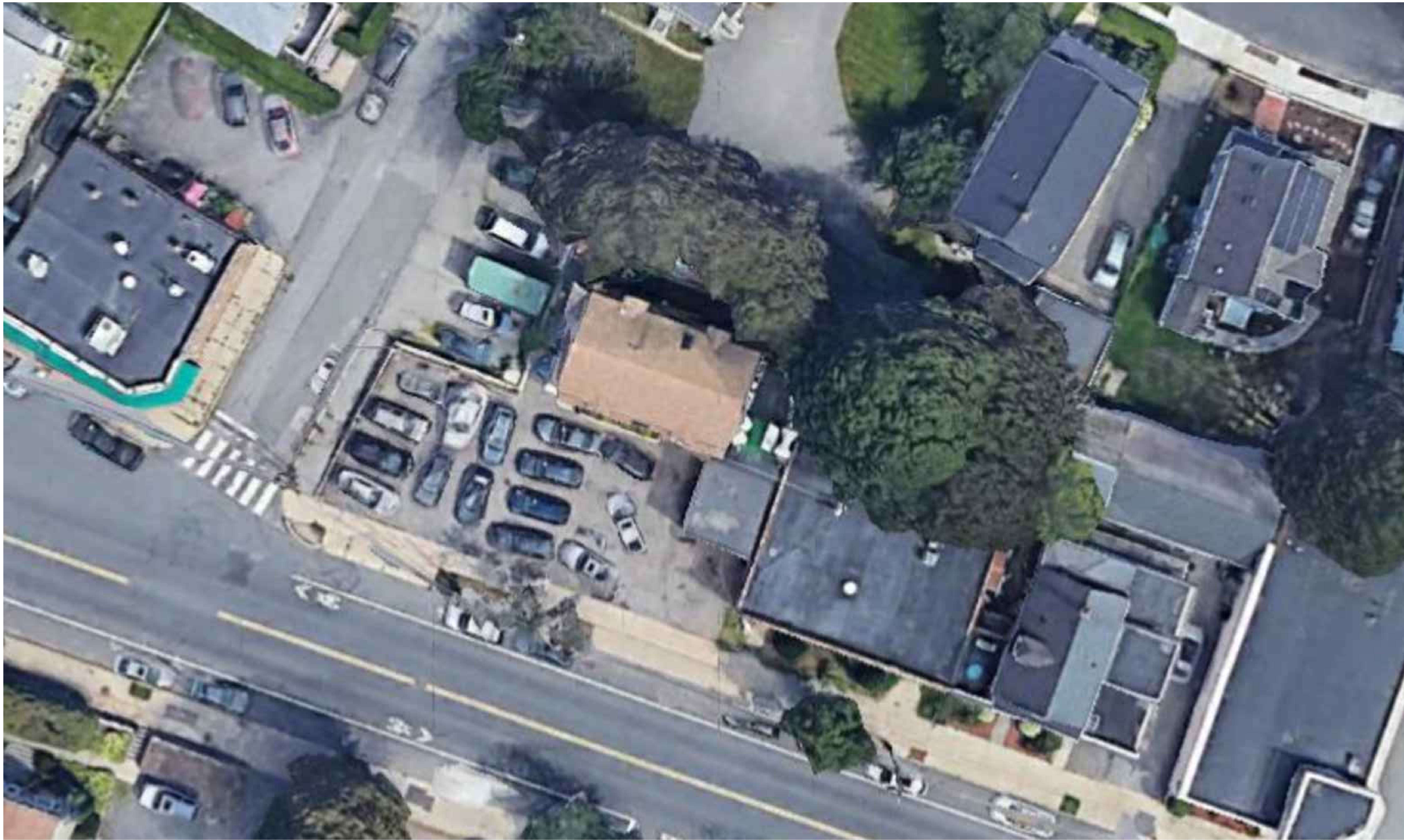
## LOCUS PLAN



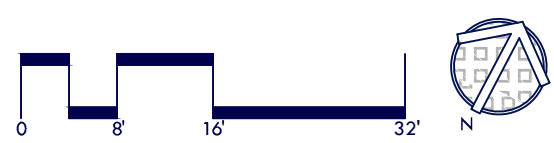
## DRAWING LIST

- ARCHITECTURAL**
- COVER SHEET/LOCUS PLAN/ZONING SUMMARY
  - EXISTING CONDITION DIAGRAM
  - PROPOSED PLOT PLAN
  - SITE PLAN/LANDSCAPING PLAN
  - LOWER LEVEL FLOOR PLAN
  - MAIN LEVEL FLOOR PLAN
  - SECOND & THIRD FLOOR PLAN
  - FOURTH FLOOR PLAN
  - BUILDING ELEVATIONS
  - RENDERINGS/VIEW FROM MASSACHUSETTS AVENUE
  - RENDERINGS/VIEW FROM CLARK STREET
  - RENDERINGS/BIRDS EYE VIEW FROM MASSACHUSETTS AVENUE
  - SHADOW STUDY/SUMMER SOLSTICE
  - SHADOW STUDY/WINTER SOLSTICE
  - SHADOW STUDY/AUTUMN EQUINOX
  - SHADOW STUDY/SPRING EQUINOX

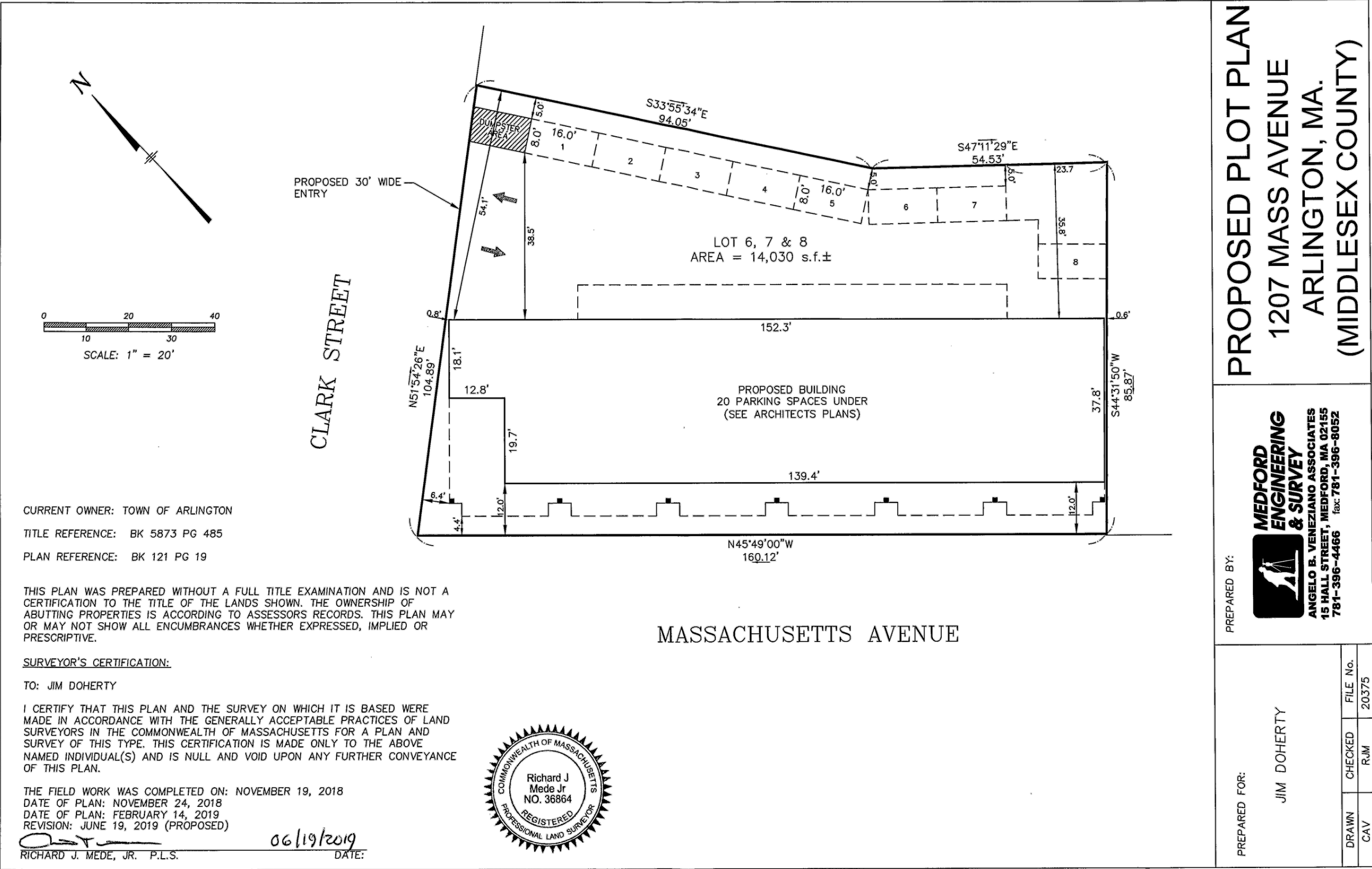




Existing Conditions Diagram





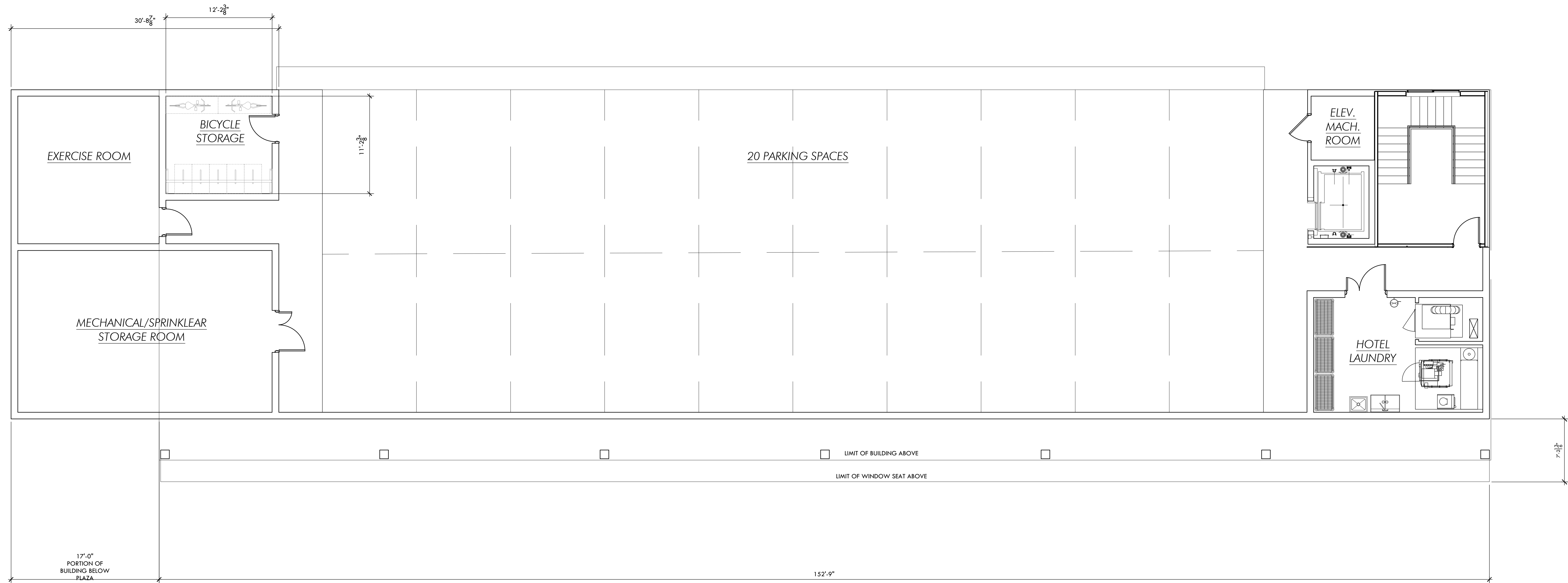




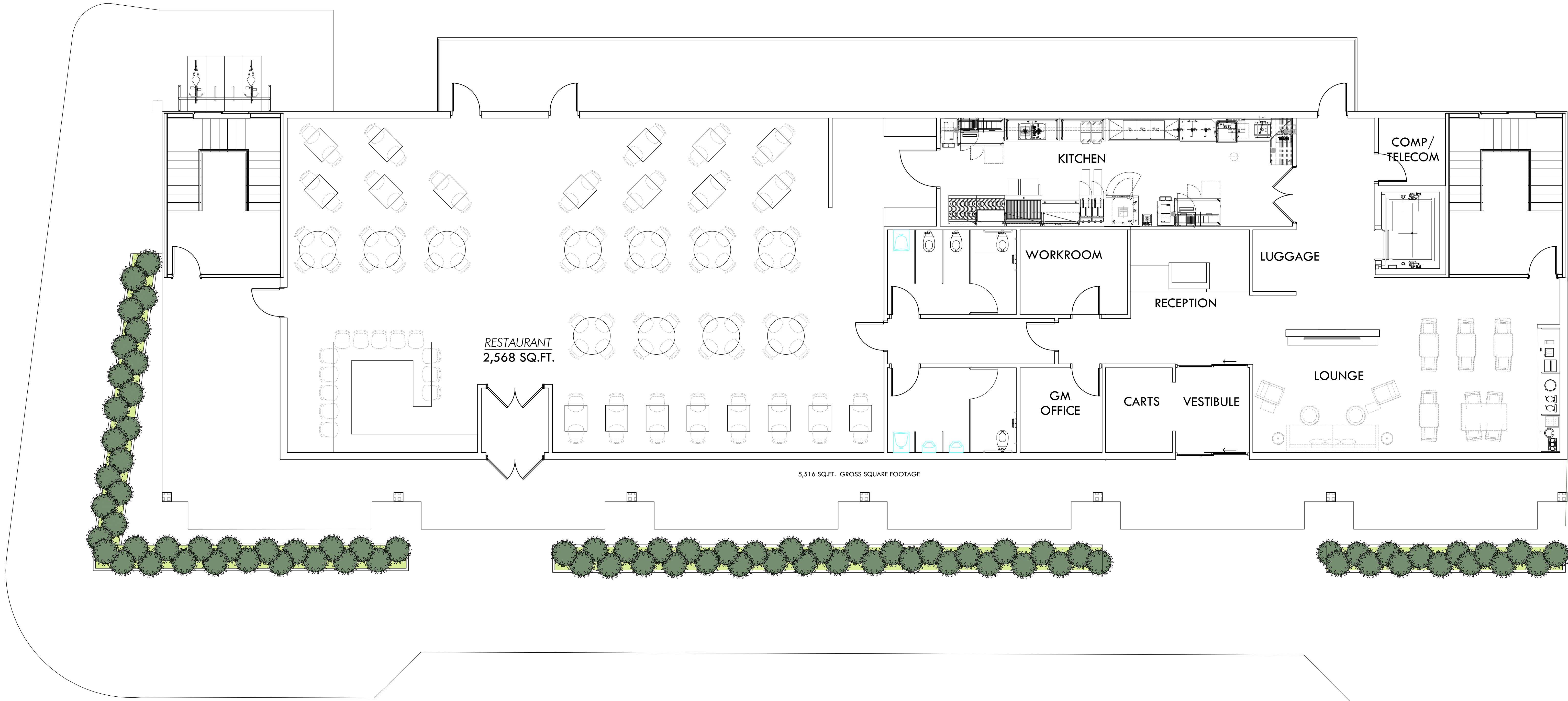


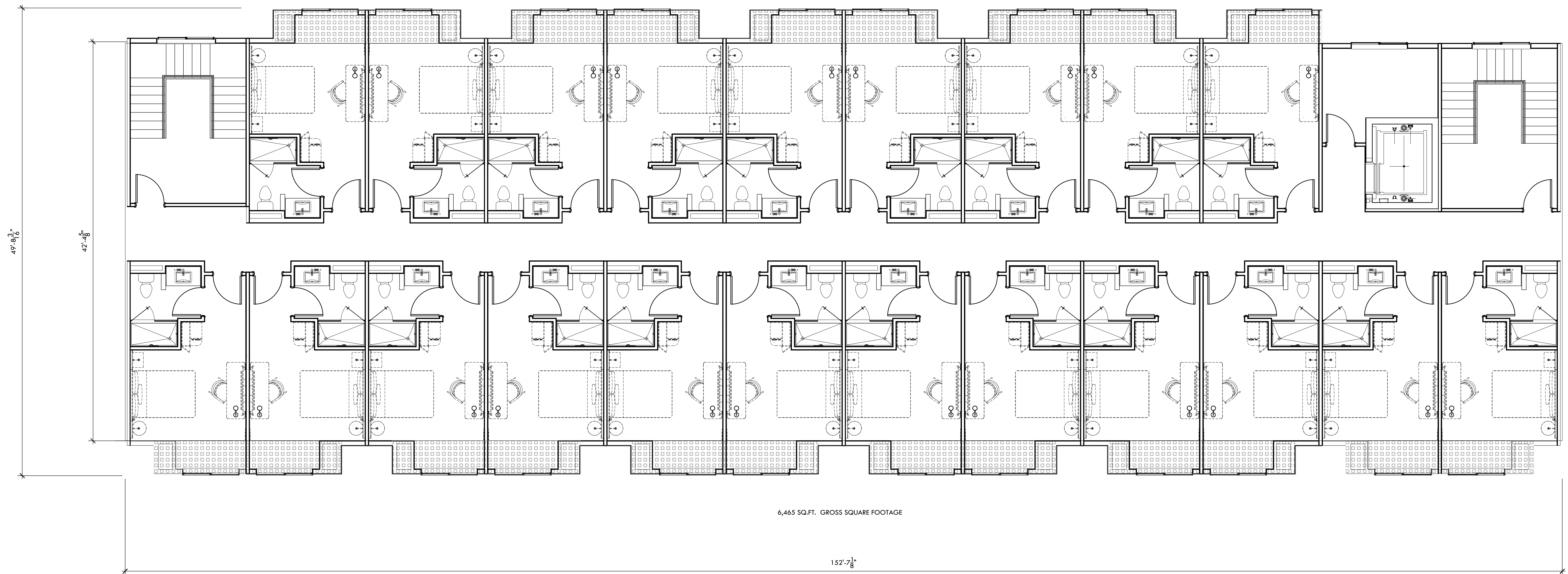
Planting Schedule				
1211 Massachusetts Ave				
75	Buxus Green Velvet	Green Velvet Boxwood	15-18"	Front
75	Carex Blue Zinger	Blue Zinger Grass	n/a	Front
4	Syringa Reticulata Ivory Silk	Tree Liliac	2"	Rear
10	Thujastandishi x Pucata	Green Grant Arborvitae	6-8'	Rear
5	Ilex x Meserveae	Blue Princess	8-10"	Rear
4	Hydrangea Quercifolia	Oakleaf Hydrangea	4'	side



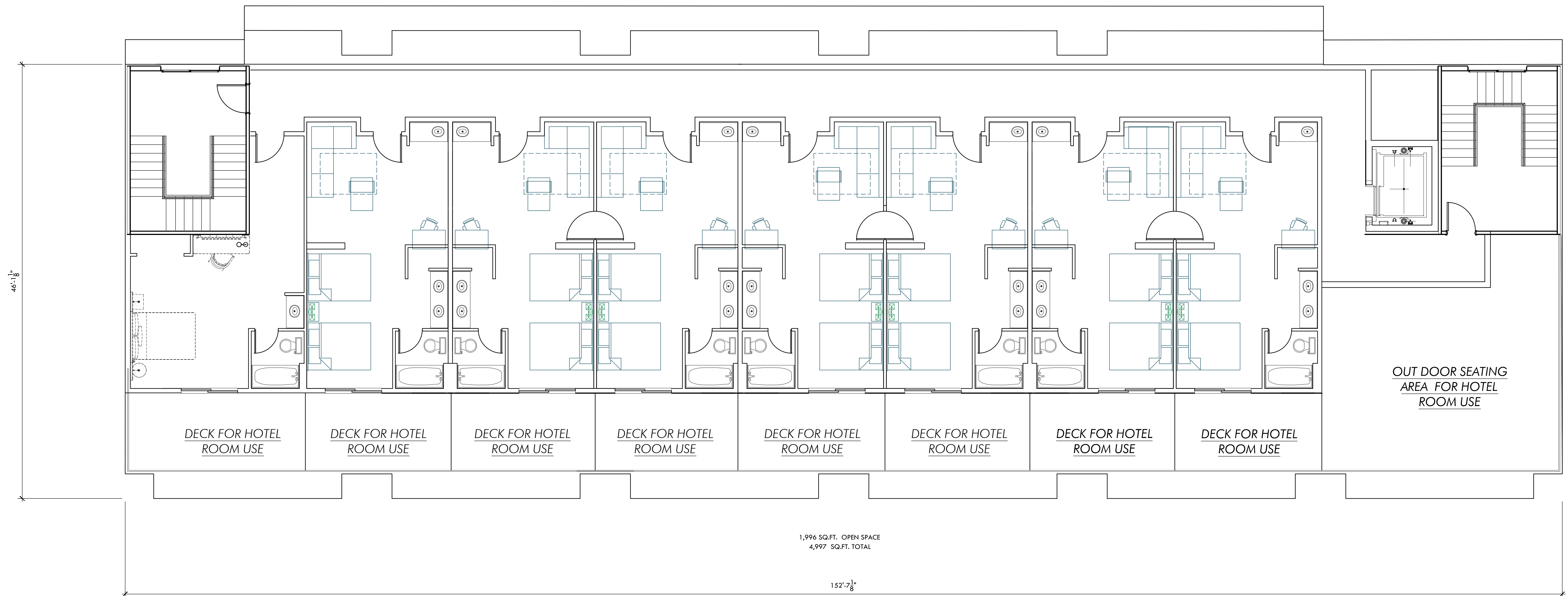








## Second & Third Floor Plan







Front Elevation (Massachusetts Avenue)



Side Elevation (Clark Street)



Rear Elevation

Building Elevations





View From Massachusetts Avenue





View From Clark Street

## Renderings





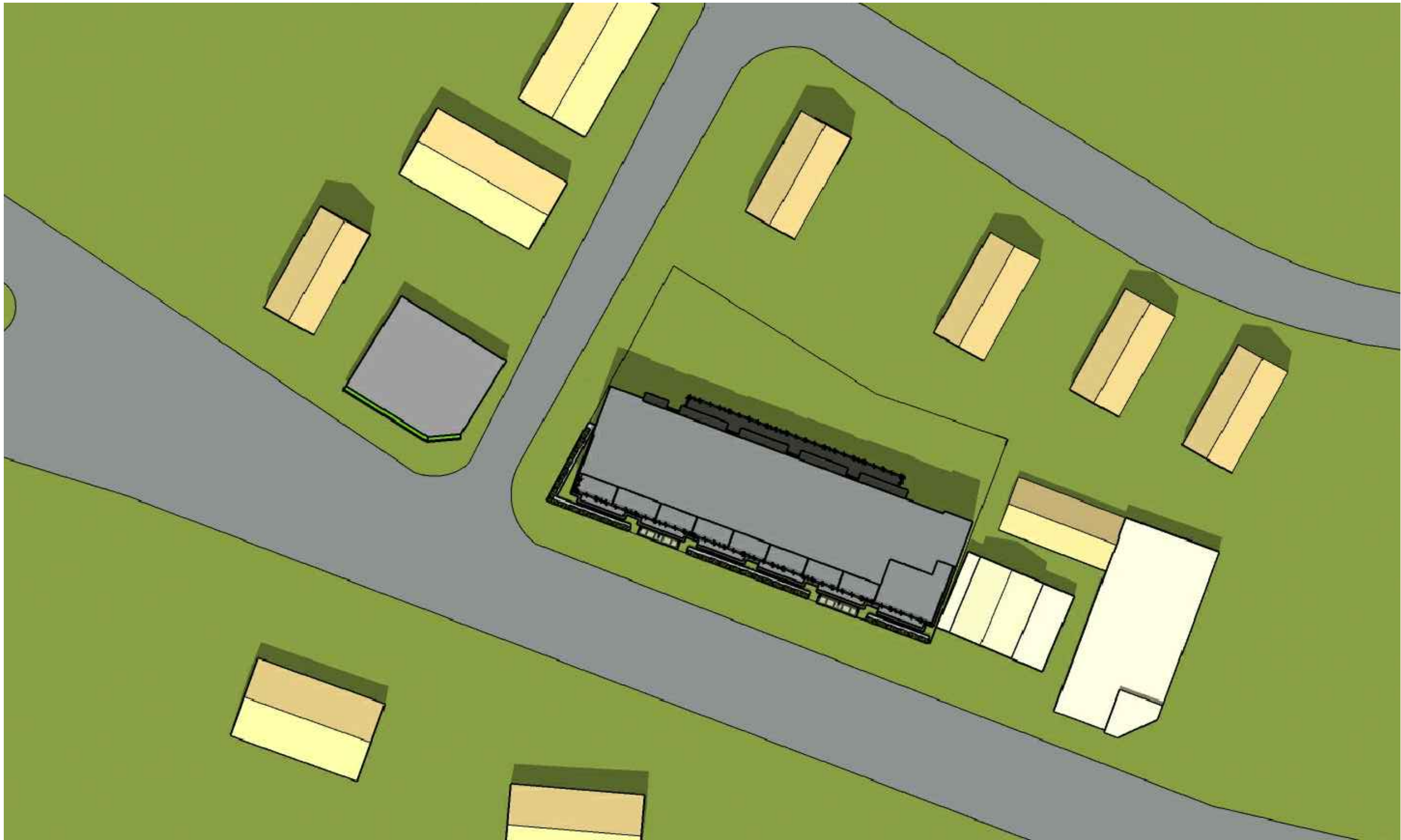
Birds Eye View From Massachusetts Avenue



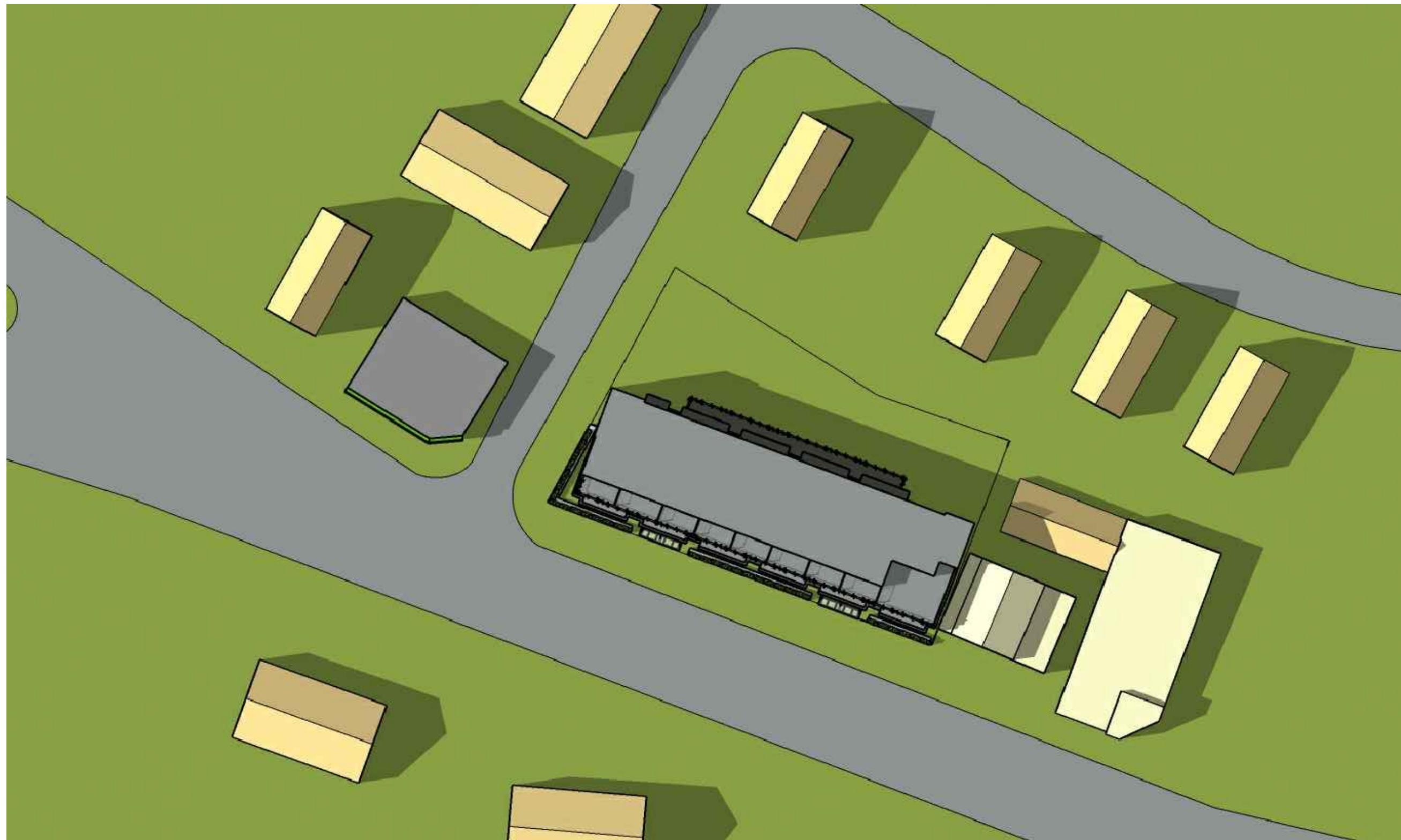
Summer Solstice



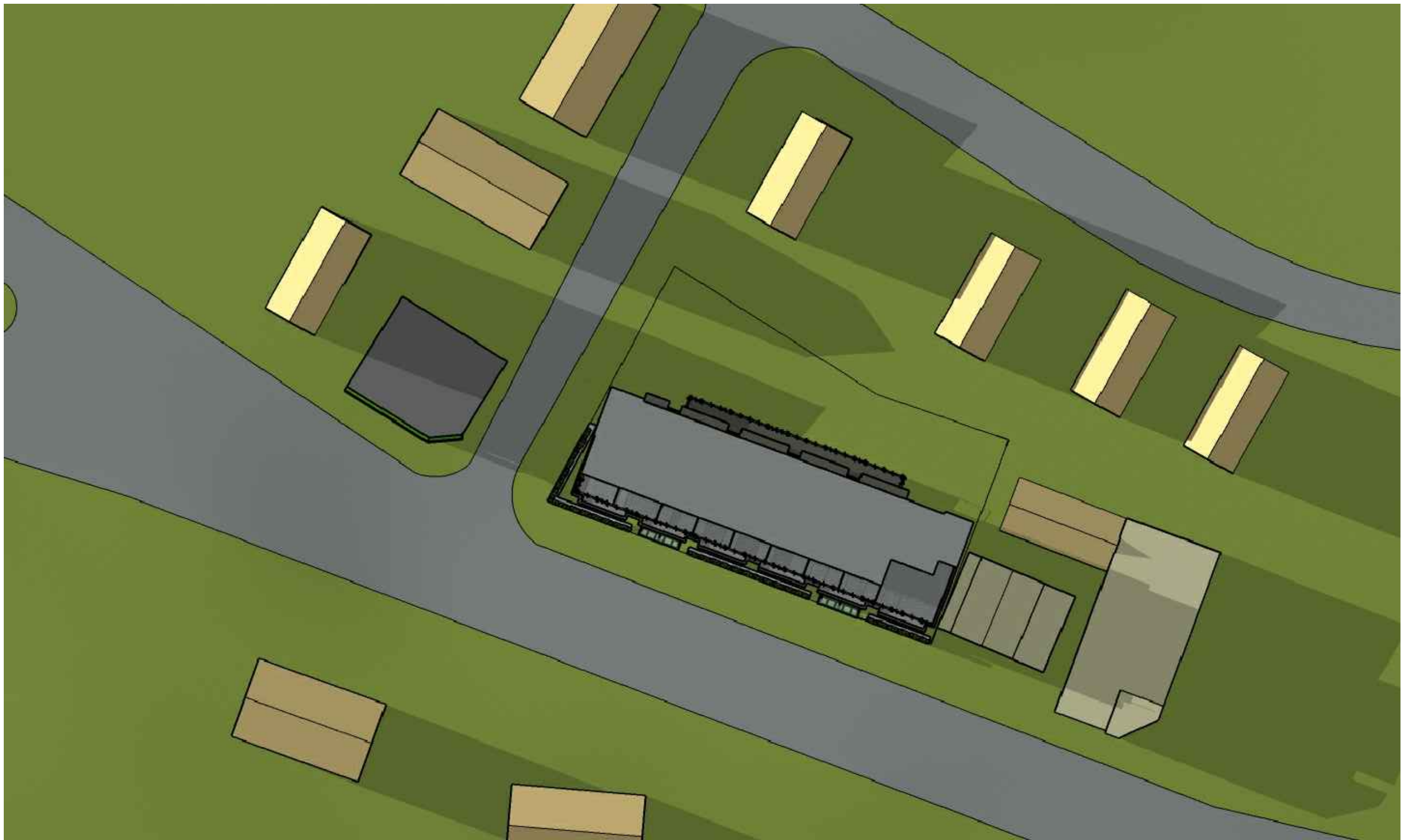
9:00 AM



12:00 PM



3:00 PM



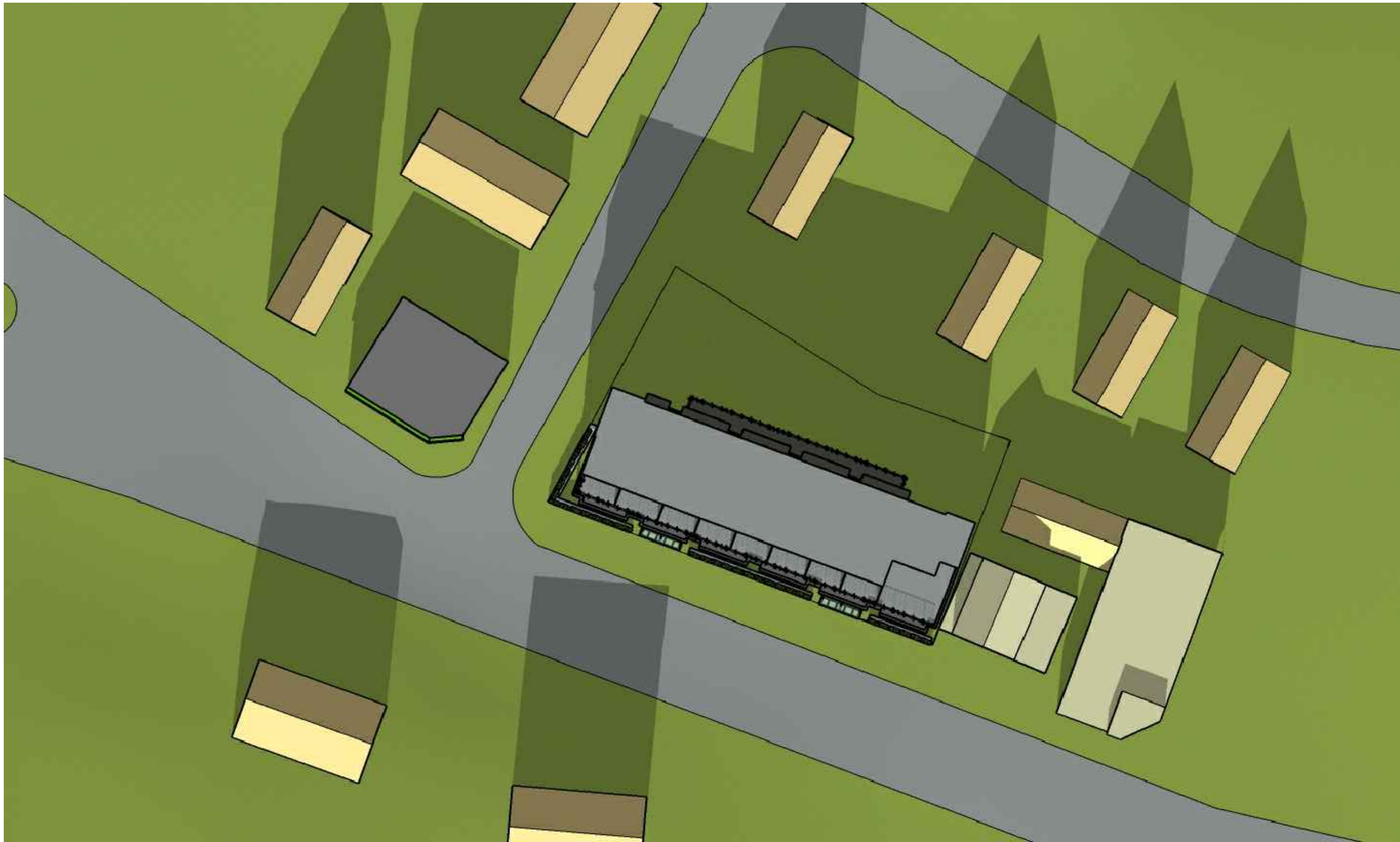
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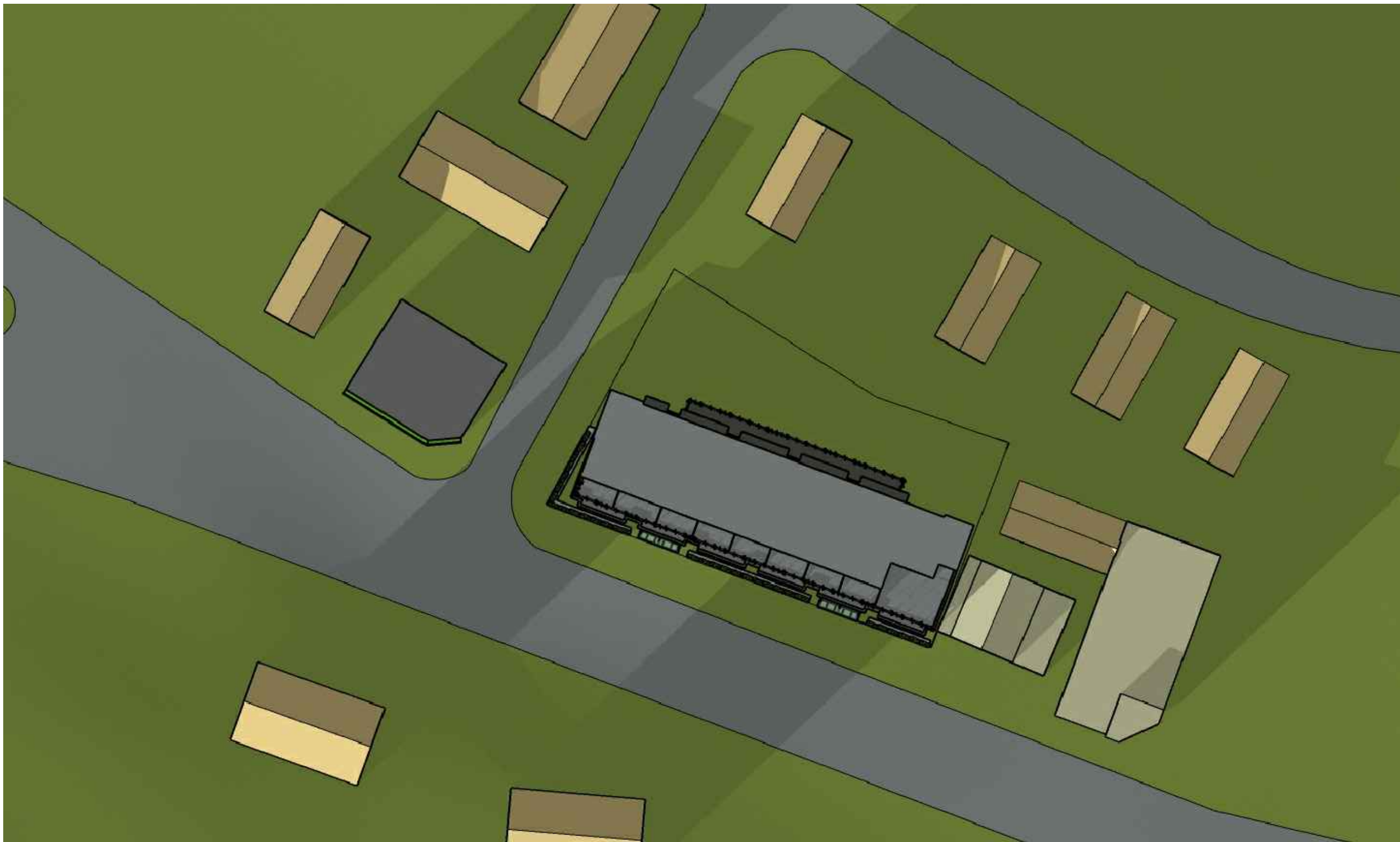
Winter Solstice



9:00 AM



12:00 PM



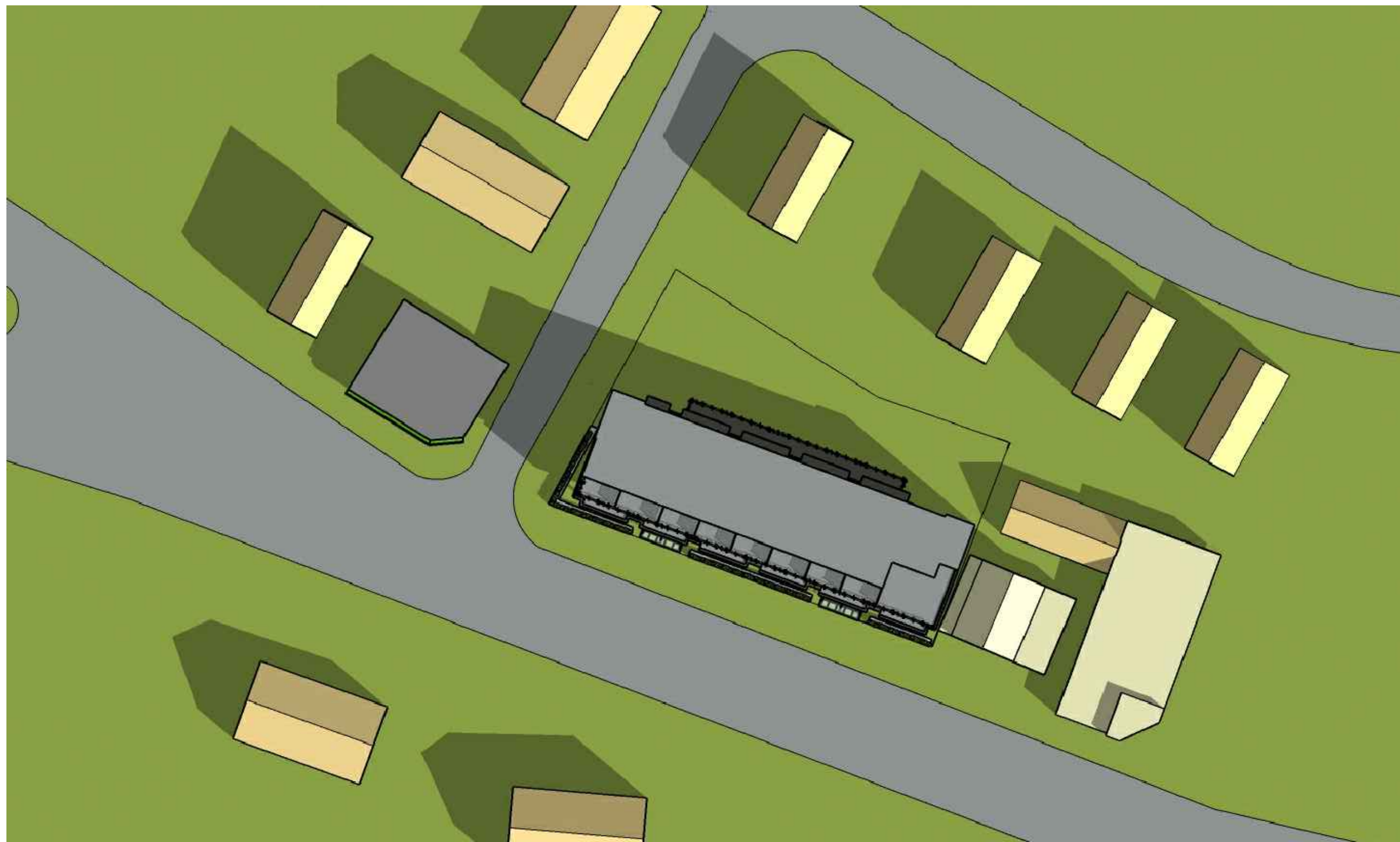
3:00 PM



6:00 PM



Autumn Equinox



9:00 AM



12:00 PM



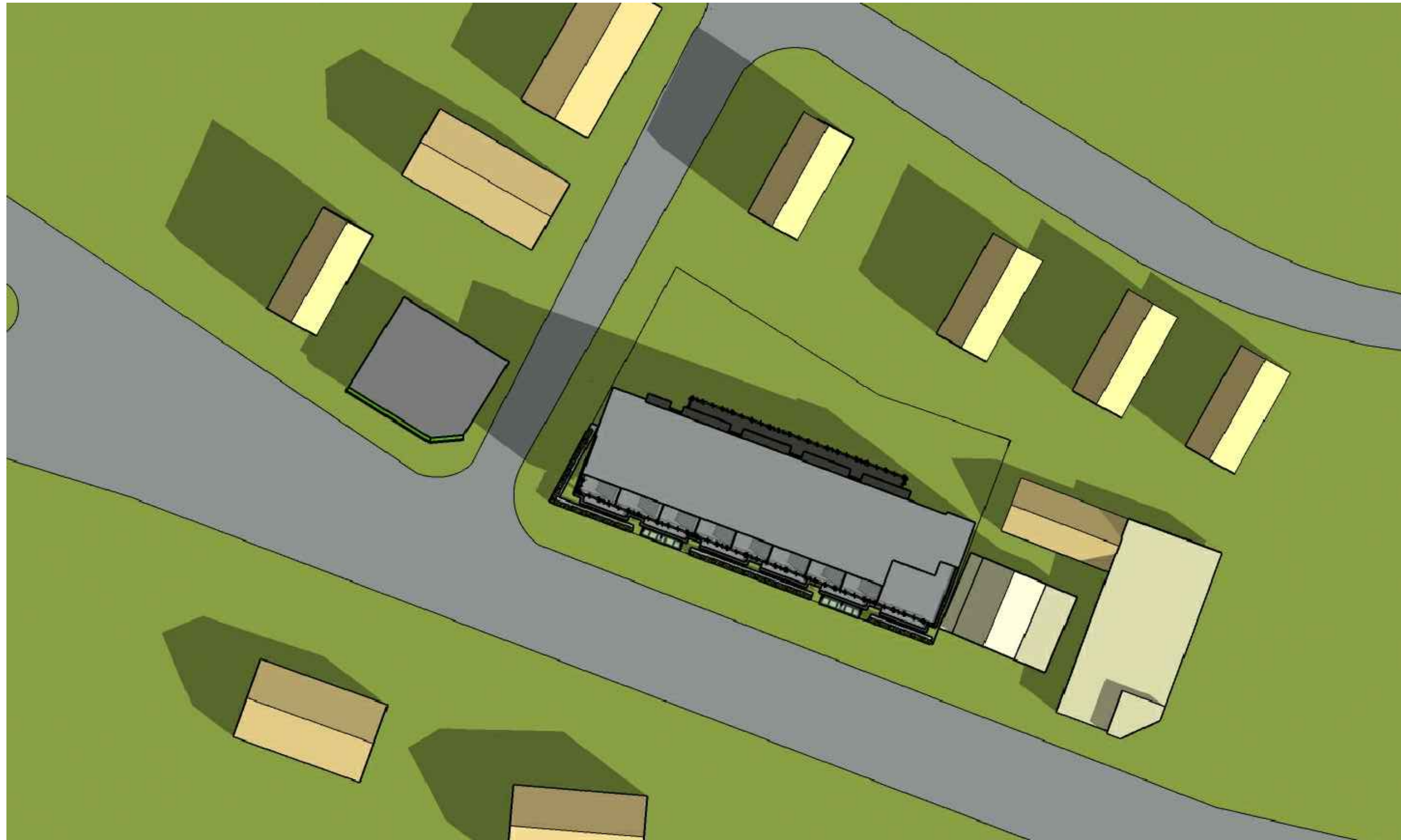
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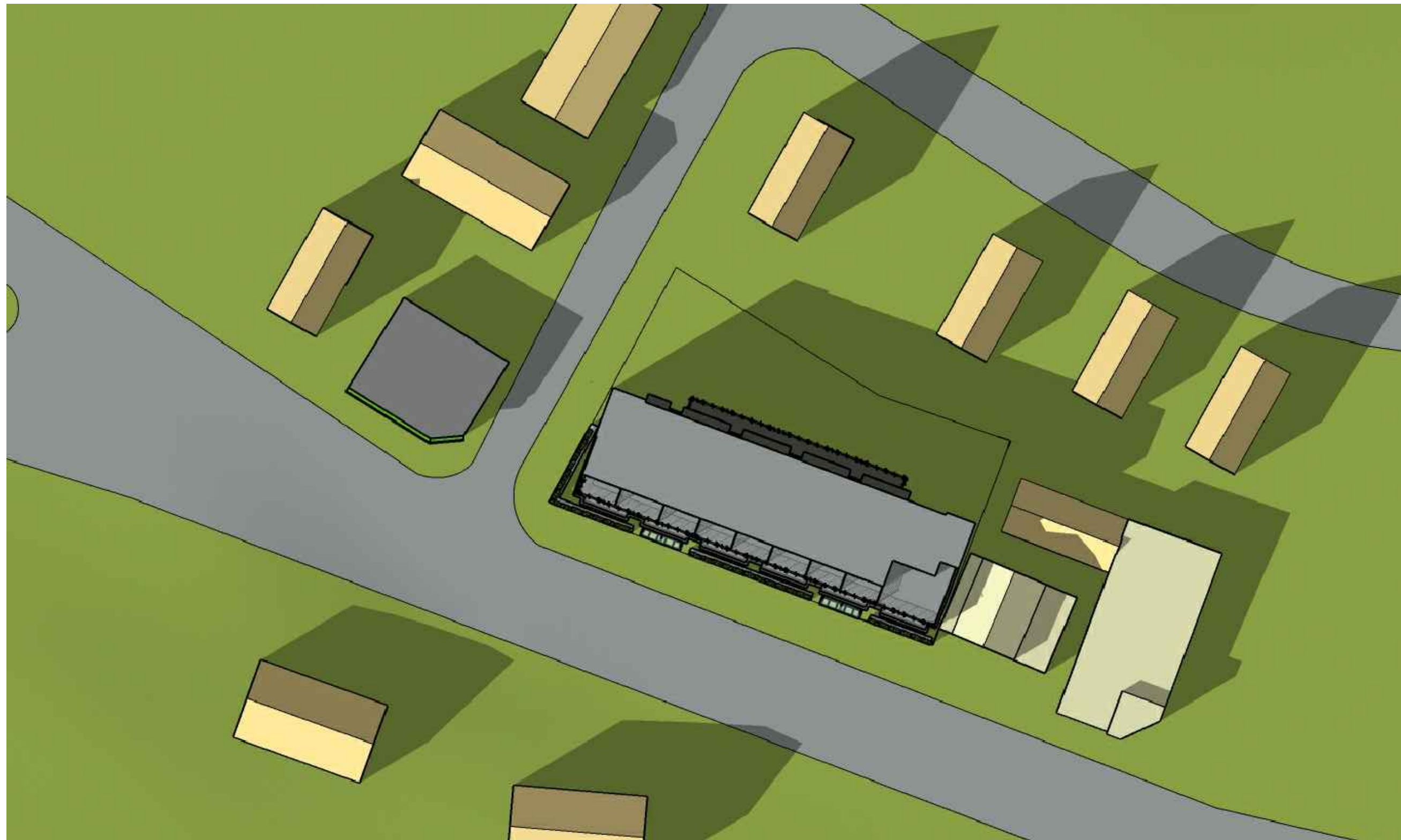
Spring Equinox



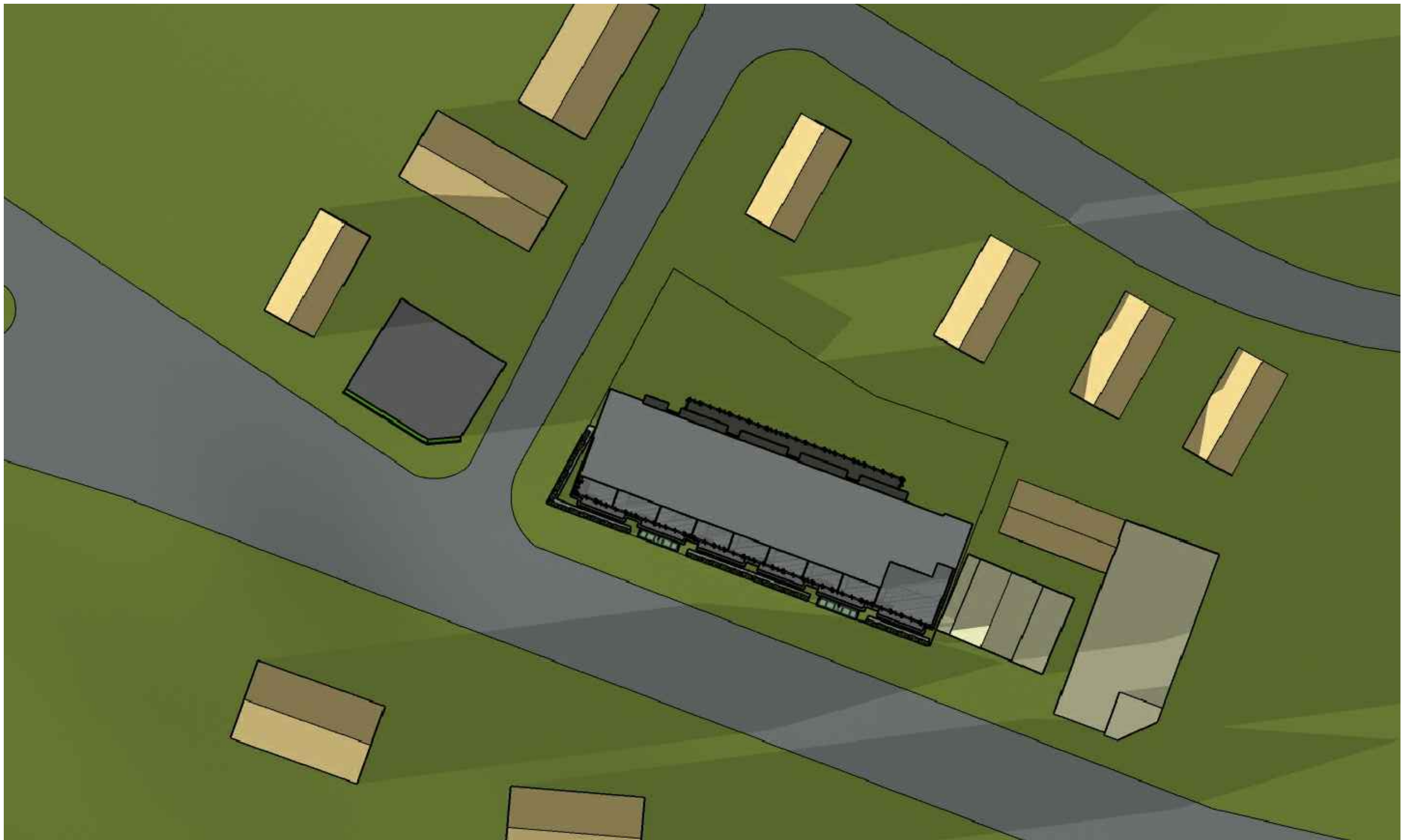
9:00 AM



12:00 PM



3:00 PM



6:00 PM





**Town of Arlington, Massachusetts**  
Department of Planning & Community Development  
730 Massachusetts Avenue, Arlington, Massachusetts 02476

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**Public Hearing Memorandum**

*The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.*

**To:** Arlington Redevelopment Board

**From:** Jennifer Raitt, Secretary Ex Officio

**Subject:** Environmental Design Review, 1207-1211 Massachusetts Avenue, Arlington, MA  
Docket #3602

**Date:** July 16, 2019

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**I. Docket Summary**

This is an application by James F. Doherty for 1211 Mass Ave Realty Trust to construct a mixed-use structure at 1207-1211 Massachusetts Avenue within the B2 Neighborhood Business District and the B4 Vehicular Oriented Business District. The Special Permit is to allow the Board to review and approve the proposed project, under Section 3.4, Environmental Design Review.

Following the Town's Request for Proposals (RFP) process in 2016, the applicant has entered into a Purchase & Sale (P&S) Agreement to purchase the property at 1207 Massachusetts Avenue in order to construct the mixed-use building, which is desirable to the Town. 1207 Massachusetts Avenue is the location of the now closed Disabled American Veterans (DAV) club, which ceased operations and has been vacant since mid-2014. The applicant currently owns the immediately adjacent property at 1211 Massachusetts Avenue, and upon successful permitting, will combine the two properties for a unified mixed-use development.

The RFP sought proposals for the purchase and future use of the parcel as a mixed-use development consistent with 2016 amendments to the Arlington Zoning Bylaw, that defined mixed-use as "[a] Combination of two or more distinct land uses, such as commercial, lodging, research, cultural, artistic/creative production, artisanal fabrication,

residential in a single multi-story structure to maximize space usage and promote a vibrant, pedestrian-oriented live-work environment.” The applicant proposes a 50-room hotel and restaurant consistent with this definition of mixed-use.

The application also requests a parking reduction under Section 6.1.5 and additional gross floor area under Section 5.3.6.

Materials submitted for consideration of this application:

- Application for EDR Special Permit,
- Narrative,
- Site Plan, Floor Plans, Elevations, and Renderings dated June 20, 2019;
- Planting Schedule;
- Parking and Bicycle Schedule;
- Shadow Study dated June 20, 2019; and,
- Traffic Demand Management Plan.

## II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

### 1. Section 3.3.3.A.

**The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.**

The applicant proposes a mixed-use structure consisting of a 50-room hotel and restaurant. Mixed-use, which as defined by the Zoning Bylaw includes lodging and commercial uses, requires a Special Permit in both the B2 Neighborhood Business District and the B4 Vehicular Oriented Business District. Mixed-use is additionally subject to Environmental Design Review under Section 3.4.G and due to the proposal’s location on Massachusetts Avenue. Mixed-use is described as being allowed in Section 5.5.1 for both the B2 and B4 Districts, in particular in the B4 District when automotive-oriented uses close and are redeveloped. The Board can find that these conditions exist for the proposed project site.

### 2. Section 3.3.3.B.

**The requested use is essential or desirable to the public convenience or welfare.**

The redevelopment of the DAV site and the adjacent outdated automotive use is desirable for the public convenience and welfare. The mixed-use structure, which combines a small boutique hotel and restaurant, is well-positioned to take advantage of tourism opportunities along the Battle Road Scenic Byway, the approximate path the British used at the beginning of the American Revolution, in Arlington and neighboring communities. A hotel and restaurant in the immediate area could be desirable to tour groups that want more personalized accommodations. There is an economic benefit that would be gained through the hotel/motel tax (6%) and meals tax (0.75%). Based on current tax revenue generated by the one hotel in Town, this

50-room hotel may generate up to approximately \$150,000 of additional tax revenue to the town on an annual basis.<sup>1</sup> The proposed hotel's proximity to Lexington, which welcomes over 100,000 tourists per year, makes it well-positioned to absorb some of the local and regional heritage and business travel, which would provide an economic benefit to the Town of Arlington and local businesses. Neighborhood residents have voiced the critical importance for more restaurants and mixed-use based on feedback gathered from residents as well as a market demand analysis that were part of the development of the Arlington Heights Neighborhood Action Plan.

**3. Section 3.3.3.C.**

**The requested use will not create undue traffic congestion or unduly impair pedestrian safety.**

The application materials do not provide detailed information regarding the traffic impact of the new use. The ARB must request additional information from the applicant on the following topics before determining that this criterion is satisfied.

The project's only means of ingress and egress is on Clark Street as the existing curb cuts on Massachusetts Avenue will be closed. Due to the new uses, a trip generation analysis is needed to understand the traffic flow and circulation of using Clark Street as the main point of access to the property. For vehicles exiting the property, turning right directs those vehicles into a residential neighborhood and a circuitous route back to Massachusetts Avenue or to Forest Street. The best course of action may be to require vehicles exiting the property to turn left onto Clark Street and then continue either north or south on Massachusetts Avenue, and the ARB will want to consider this as a condition of a decision. By adding more turning traffic to the intersection of Clark Street and Massachusetts Avenue there may be the need to address pedestrian safety at this intersection. On the opposite side of the street is an inbound MBTA Route 77 and 79 bus stop with departures every few minutes, so a cross walk may be necessary at the intersection as the closest cross walk is at Appleton Street. However, without a trip generation analysis, the ARB does not have the full scope of understanding regarding additional traffic as a result of the proposed project.

The nearby intersection of Appleton Street and Massachusetts Avenue is uncontrolled except for when a pedestrian triggers a red light in order to cross the street. A large majority of the pedestrians at this intersection are students walking to or from the Ottoson Middle School. More information is needed from the applicant on how the introduction of a hotel and restaurant could affect the operation of this intersection, especially during the beginning and end of the school day during the school year.

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<sup>1</sup> According to the Town of Lexington's most recent Economic Development Report to Town Meeting, the Town of Lexington generates an average of \$1.27 million dollars of revenue in hotel/motel taxes.

Additionally, the Transportation Demand Management (TDM) Plan submitted in support of the parking reduction request needs firm commitments regarding the methods in which vehicular use will be reduced at the property. The applicant should also consider providing staff subsidized transit passes and guaranteed rides home. Commitments such as these must be required in any future lease of the building. Finally, the plans show an area to pull off of Massachusetts Avenue which could facilitate valet parking, and could be supported, but this would require approval from the Select Board.

It should be noted that the proposal will improve pedestrian safety along the project site's Massachusetts Avenue frontage. Two large curb cuts will be closed as access to the property will be from Clark Street, where the curb cut will be narrowed.

**4. Section 3.3.3.D.**

**The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.**

The mixed-use structure introduces different uses than presently on the project site. There may be different demand on the municipal systems as a result, but will not create hazards affecting health, safety, or the general welfare of the immediate area or in any other area of the Town. While the application materials note that a stormwater system will be installed to control roof and surface stormwater runoff, the ARB will need more information regarding water and sewer usage. The applicant should submit evidence that the public water, drainage, and sewer system are capable of handling the needs of the 50-room hotel and restaurant.

**5. Section 3.3.3.E.**

**Any special regulations for the use as may be provided in the Bylaw are fulfilled.**

No special regulations are applicable to the proposal. The Board can find that this condition is met.

**6. Section 3.3.3.F.**

**The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.**

The 2016 Annual Town Meeting adopted mixed-use zoning for all business districts with an affirmative vote of 187-35. This stretch of Massachusetts Avenue does not have a distinct aesthetic and there are no predominant architectural styles that characterize this area. The proposed mixed-use structure will not impair the integrity of the district and will provide connections between the Arlington Heights business district and other segments of the Mass Ave commercial corridor. The hotel use in particular will provide greater access for tourists to Arlington's historic

resources that make it part of the Battle Road Scenic Byway, including the nearby Foot of the Rocks monument and the Old Schwamb Mill.

The immediate area around the project site is a mix of residential and commercial spaces. Immediately behind the project site is a neighborhood of mostly single- and two-family homes in an R2 Two-Family District. Higher density residential uses are present across the street on Massachusetts Avenue, but the R2 District carries across Massachusetts Avenue as well where significant elevation is gained. The Heights business district is a short distance away (about 1,500 feet to the west) and an industrial-zoned area is less than 1,000 feet to the east.

**7. Section 3.3.3.G.**

**The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.**

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find that this condition is met.

**III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)**

**1. EDR-1 Preservation of Landscape**

**The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.**

The existing site condition is primarily impervious, but the proposal will increase the amount of open space on the site. A 5-foot landscaped buffer is provided along the rear property line that will be planted with tree lilacs, arborvitae, and smaller shrubs such as hydrangea and holly. Planters along Massachusetts Avenue are also proposed. While a planting schedule is provided, a landscape plan must be submitted. The application materials indicate that there will be 1,581 square feet of landscaped open space and 3,384 square feet of usable open space. The landscape plan should also document where the two types of open space will be satisfied on the property.

**2. EDR-2 Relation of the Building to the Environment**

**Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.**

At 4 stories and 44 feet tall, the proposed building is taller than most of the buildings in the immediate vicinity. On the opposite side of Massachusetts Avenue, the terrain



quickly gains elevation, so nearby buildings appear much taller due to the elevation change. The proposal also steps in the first floor 8 inches from the second and third floor, and provides the upper-story step back at the top of the third floor at 34 feet. Section 5.3.17 requires that building more than three stories in height, such as the proposal, an additional 7.5-foot step-back (upper story building setback) shall be provided beginning at the third story level or 30 feet above grade, whichever is less. As part of the EDR jurisdiction, these requirements should be further addressed until the Board is satisfied that the building is well-situated on the parcels.

The building does not trigger the height buffer area of Section 5.3.19 because it is proposed at the lower maximum stories and height as identified in the Table of Dimensional and Density Requirements for the Business Districts. However, the application materials also provide a shadow study during each season at the respective Solstice and Equinox.

**3. EDR-3 Open Space**

**All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.**

As noted above, the proposed project will provide open space on the existing primarily impervious site. The application materials indicate that there will be 1,581 square feet of landscaped open space and 3,384 square feet of usable open space. Landscaped buffers will be located at the rear of the property providing some relief to the residential structures located behind the project site. A large patio along Massachusetts Avenue is proposed, which can create gathering space and an inviting atmosphere along the sidewalk. A landscape plan must be submitted and must document where the two types of open space will be satisfied on the property in order to assess compliance with this criterion.

**4. EDR-4 Circulation**

**With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.**

The application materials indicate that 28 parking spaces will be provided on the site, either under the building or at the rear of the property. The applicant has requested a parking reduction under Section 6.1.5:

<b>Parking Requirement</b>			
		<u><b>Zoning Requirement</b></u>	<u><b>Total Parking Required</b></u>
<b>Hotel</b>	<b>50 rooms</b>	<b>1 space per room</b>	<b>50</b>
<b>Restaurant</b>	<b>2,568 sf</b>	<b>1/300 sf*</b>	<b>0</b>
<b>Total Parking</b>			<b>50</b>
<b>Section 6.1.5 Reduction</b>			<b>Up to 25% of the requirement, or 13 spaces</b>
<b>Total Parking Provided</b>			<b>28</b>
* First 3,000 sf of non-residential space in mixed-use projects is exempt.			

In general, as discussed under the response to criterion 3.3.3.C, there is no information about circulation on and around the project site. The only access to the property is from Clark Street and there is no information on how trips to and from the project site will change. A trip generation analysis is needed to understand the traffic flow and circulation of using Clark Street as the main point of access to the property. A trip generation analysis may indicate that right turns from the property onto Clark Street should be restricted and there may be the need for pedestrian improvements at the intersection of Clark Street and Massachusetts Avenue due increased traffic.

On the project site, there is no information on where loading and unloading will occur. Based on the application materials, there is no information on the size of truck that can access the project site and whether vendors need to be limited to a certain size truck in order to navigate Clark Street and the parking lot. Additionally, the floor plan does not seem to provide direct loading access to the restaurant's kitchen or the hotel from the rear parking lot. If loading and unloading will occur on Massachusetts Avenue, it is not clear whether there is shoulder space for a large truck to park during these activities. To combat idling and disruption to the surrounding neighborhood, deliveries should be limited to certain hours of the day.

Circulation within the parking lot is not clearly discussed. Some of the parking spaces provided are tandem spaces and it is not clear how the spaces will be assigned or allocated between the hotel and restaurant. If the parking spaces will be used primarily by the hotel, the ARB needs an understanding of the on-street parking utilization of the area. Additionally, there is no information in the applicant materials regarding the safety and security of the proposed parking area other than 12-foot

light poles. It will be important for the ARB to understand how the spaces will be utilized on the property.

Additionally, the Transportation Demand Management (TDM) Plan submitted in support of the parking reduction request needs firm commitments regarding the methods in which vehicular use will be reduced at the property. The Applicant could also consider providing staff subsidized transit passes and guaranteed rides home. Commitments such as these must be required in any future lease of the building. Finally, the plans show an area to pull off of Massachusetts Avenue which could facilitate valet parking, and could be supported, but this would require approval from the Select Board.

It should be noted that the proposal will improve pedestrian safety along the project site's Massachusetts Avenue frontage. Two large curb cuts will be closed as access to the property will be from Clark Street, where the curb cut will be narrowed.

The application materials indicate that proposal exceeds the requirements of the newly adopted bicycle parking bylaw. For the mixed-use building, 5 short-term bicycle parking spaces are required and 2 long-term bicycle parking spaces are required. The proposal exceeds this requirement by providing 7 short-term spaces and 7 long-term spaces. However, the application materials do not provide any specifications of the proposed racks, and the location of the short-term spaces is inconsistent between the plan set and renderings and the written information. The ARB should request additional information.

**5. EDR-5 Surface Water Drainage**

**Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.**

**In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.**

**The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.**

The application materials only indicate that a subsurface infiltration system will be provided under the parking lot to control surface and roof runoff. There are no further details provided in the application materials. The applicant must submit an engineered site plan showing surface water drainage systems and a stormwater management plan that includes an analysis that will inform the size of an underground infiltration system and includes engineering plans for the system. It is also strongly recommended that the applicant include low impact development techniques such as creating a rain garden or other similar feature in the landscape area in the northeast corner of the property.

**6. EDR-6 Utilities Service**

**Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.**

The application materials indicate that the new utilities will be underground, but the ARB will want additional information from the applicant on whether any of the existing utilities that serve the site will be reused. It should be noted that there are three utility poles (one of which is a double pole) along the Massachusetts Avenue frontage. Although requests to move or consolidate utility poles are often not accepted by the utility companies, the applicant should attempt to coordinate with the utility company to at least remove the double pole and consolidate the operations to the other two poles as the poles and lines interfere with the structure's visibility. The ARB will want to understand that the services carried on these poles will not be overloaded.

**7. EDR-7 Advertising Features**

**The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.**

The signage proposed in the application materials are place holders for the mixed-use structure. However, the proposal appears to be consistent with the newly adopted sign bylaw in terms of location and size. The application materials indicate that the signage will be back lit, but there is no information in the application materials about lighting of the building in general. A condition of a decision by the ARB should include a requirement that the final signage be reviewed for compliance.

**8. EDR-8 Special Features**

**Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.**

There will be equipment on the roof to service the mixed-use structure, and it appears that some of the equipment will be screened. Each hotel room has its own system and the louvers can be seen on the renderings. Although Arlington does not specify a certain noise level at the property line, many nearby communities identify a day-time noise level of no more than 65 dbA or no more than 10 dbA over the background noise level. Overnight, many nearby communities identify a noise level of 50 dbA. Using this as guidance, the applicant should clarify the noise impact of the HVAC and other noise-emitting equipment.

To reduce noise from deliveries or from solid waste removal, the ARB will want information on anti-idling measures and time of day restrictions to ensure that these services do not impact the surrounding residential properties.

The applicant should clarify how the dumpster will be screened and shared.

**9. EDR-9 Safety**

**With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.**

As noted in the application materials, the proposed interior layout plans have been designed to facilitate building evacuation and accessibility by fire, police, and other emergency personnel and equipment. The application materials indicate that the rear parking lot will be illuminated through the use of 12-foot pole mounted LED lights; however, there is no indication on the plans where these light poles would be located and the specification of such. Further, there is no information on how the open garage will be secured.

**10. EDR-10 Heritage**

**With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.**

The existing structures are not listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* nor are they under the jurisdiction of

the Arlington Historical Commission. As such, the site contains no historic, traditional or significant uses, structures or architectural elements. The Board can find that this condition is met.

Two properties on the opposite side of Massachusetts Avenue (1210 Massachusetts Avenue and 1218-1222 Massachusetts Avenue) are under the jurisdiction of the Historical Commission. The redevelopment of the subject property will not disrupt historic, traditional, or significant uses, structures, or architectural elements that exist on the adjacent properties.

#### **11. EDR-11 Microclimate**

**With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.**

There are no proposed changes that will impact the microclimate. A shadow study was prepared and is provided in the application materials to illustrate how the building may create additional shadows in the immediate area. Although the project does not trigger the height buffer area, the ARB will want to assess to ensure that the Board is satisfied that the building is well-situated on the parcels.

#### **12. EDR-12 Sustainable Building and Site Design**

**Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.**

The Applicant indicates that the building will meet the Stretch Code. Additional information regarding the LEED Checklist is needed.

### **IV. Conditions**

#### **General**

1. The final design, sign, exterior material, landscaping, and lighting plans shall be subject to the approval of the Arlington Redevelopment Board at the time when future operators are identified. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.
3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.
4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.
5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with Article 30 of Town Bylaws.
6. The Applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the development. The applicant shall provide evidence that a final plan for drainage and surface water removal has been reviewed and approved by the Town Engineer.
7. Upon installation of landscaping materials and other site improvements, the Applicant shall remain responsible for such materials and improvement and shall replace and repair as necessary to remain in compliance with the approved site plan.
8. Upon the issuance of the building permit the Applicant shall file with the Inspectional Services Department and the Police Department the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.



# LEED v4 for BD+C: New Construction and Major Renovation

## Project Checklist

Project Name: **1207-1211 Massachusetts Avenue**

Date: **7/18/2019**

Y ? N

☐ ☐ ☐ Credit Integrative Process

1

2	0	0	Location and Transportation	16
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit LEED for Neighborhood Development Location	16
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Sensitive Land Protection	1
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit High Priority Site	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Surrounding Density and Diverse Uses	5
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Access to Quality Transit	5
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Bicycle Facilities	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Reduced Parking Footprint	1
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Green Vehicles	1

1	0	0	Sustainable Sites	10
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Construction Activity Pollution Prevention	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Site Assessment	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Site Development - Protect or Restore Habitat	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Open Space	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Rainwater Management	3
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<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Light Pollution Reduction	1

9	0	0	Water Efficiency	11
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Outdoor Water Use Reduction	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Indoor Water Use Reduction	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Building-Level Water Metering	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Outdoor Water Use Reduction	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Indoor Water Use Reduction	6
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Cooling Tower Water Use	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Water Metering	1

0	0	0	Energy and Atmosphere	33
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Fundamental Commissioning and Verification	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Minimum Energy Performance	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Building-Level Energy Metering	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Fundamental Refrigerant Management	Required
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<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Optimize Energy Performance	18
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Advanced Energy Metering	1
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<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Enhanced Refrigerant Management	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Green Power and Carbon Offsets	2

0	0	0	Materials and Resources	13
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Storage and Collection of Recyclables	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Construction and Demolition Waste Management Planning	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Building Life-Cycle Impact Reduction	5
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Building Product Disclosure and Optimization - Environmental Product Declarations	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Building Product Disclosure and Optimization - Sourcing of Raw Materials	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Building Product Disclosure and Optimization - Material Ingredients	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Construction and Demolition Waste Management	2

8	0	0	Indoor Environmental Quality	16
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Minimum Indoor Air Quality Performance	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Prereq Environmental Tobacco Smoke Control	Required
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Enhanced Indoor Air Quality Strategies	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Low-Emitting Materials	3
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Construction Indoor Air Quality Management Plan	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Indoor Air Quality Assessment	2
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Thermal Comfort	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Interior Lighting	2
<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Credit Daylight	3
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Quality Views	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Acoustic Performance	1

1	0	0	Innovation	6
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Innovation	5
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit LEED Accredited Professional	1

0	0	0	Regional Priority	4
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Regional Priority: Specific Credit	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Regional Priority: Specific Credit	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Regional Priority: Specific Credit	1
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Credit Regional Priority: Specific Credit	1

21	0	0	TOTALS	Possible Points: 110
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**Certified:** 40 to 49 points, **Silver:** 50 to 59 points, **Gold:** 60 to 79 points, **Platinum:** 80 to 110



To: Erin Zwirko, Assistant Director of planning and Community Development

From: James Doherty, Trustee, 1211 Mass Ave Realty Trust

Date: July 18, 2019

Subject: 1207 – 1211 Massachusetts Avenue, Environmental Design Review

Below please find some information related to the issues we have discussed. I also have reviewed the Public Hearing Memorandum dated July 16, 2019 from Jenny to the ARB members. We are in the process of preparing responses and proposed action steps relating to these comments. We will be prepared to discuss these matters at the hearing on July 22, 2019.

1. Mixed use ownership structure and economics

These types of developments generally operate in a similar manner. First, the hotel portion typically has an owner, operator and in some cases, a brand partner. In this particular case, there will most likely not be a brand, but rather an independent boutique operator. The restaurant will be operated as an independent operator.

The current owners of the property will continue to have an ownership stake in the real estate and possibly one or both of the business operations. We do not intend to participate in the active operations of either proposed use.

2. Recruitment efforts

As part of our due diligence process, we have spoken or communicated with approximately a half dozen owners/operators of hotels on a regional and national basis. The boutique hotel model is a very active and desirable style today. Operators have found them to be very successful to operate in areas such as the subject. Visitors love the upscale finish, “homey” feel and personal touch. All of the operators we spoke with expressed interest. They feel the social/economic makeup of the area and surrounding towns provide a sound base. This industry is also very competitive and time sensitive. Serious commitments develop when formal permitting is finalized.

Our discussions regarding the restaurant portion have focused on two main principles. First and foremost, we are only interested in an upscale restaurant which will provide quality food and drinks. In addition, we want something that provides a comfortable gathering place, including live music, for the residents of the Heights and visitors.

3. Comparable Boutique Hotels

As mentioned above, many “boutique” hotel projects typically are part of a mixed-use development such as this proposal. These developments have been the springboard for revitalizing older retail areas in some cases. Our initial research indicated a few in our surrounding communities:

- The inn at Hastings Park, Lexington
- Porter Square Hotel, 1924 Mass Ave., Cambridge
- Hotel 1868, 1868 Mass Ave., Cambridge
- The Groton Inn, 128 Main Street, Groton

#### 4. Economic Benefits

The direct and indirect economic benefits from this proposal should not be underestimated. Obviously, all real estate produces property tax and this proposal will substantially increase from the current improvements at 1211 Mass Ave. In addition, the DAV is currently exempt and not paying any taxes. This development will also add significant property tax “growth” while also expanding the commercial tax base.

In addition, this proposal will contribute potentially \$200,000 per year in hotel tax to the Town as well as significant meals tax.

The benefits above are the easily quantifiable economic benefits, but as we all know, there are many more economic spin offs. Below, I have outlined examples of these benefits during construction and post construction:

- We currently have relationships with Arlington supply vendors providing Hardware, building materials, plumbing supplies and professional services as well as local tradesmen. We anticipate working with these vendors on this project.
- Construction workers will be frequenting local eateries, gas stations and other professional services. We estimate that there will be an average of ten workers per day for approximately one year. Exposing the Heights to many people, who may not currently spend time here, is a great opportunity for area businesses.
- Once the project is completed, we intend to use local vendors for as many services and products as possible. It is difficult to guarantee this, but both of the proposed uses in this mixed use proposal require community involvement and we believe that, if competitive, this is a great way of building relationships in the community and developing potential customer base.

A primary statistic for retail business is the time prospective customers spend in a “shopping area” (mall, district, location). The trend in retail currently, is to add restaurants and out-door performance areas. This allows customers to relax and spend more time within a “shopping area”, resulting in more spending to businesses. This project will add to this proven concept. The performance/gathering area at the corner of Mass Avenue and Clark Street will provide an area where history, music, art exhibitions and other functions may be held for the community and visitors alike.

Part of our marketing plan is based on the Lexington tourism market. This location is ideal; it will be one of the closest hotels to “The Green”. We anticipate this being one of the best opportunities in decades to leverage tourists from Lexington to our rich History in Arlington.

## 5. Storm Water

As noted in our application, this is a “net reduction” proposal and does not trigger the storm water Bylaw. However, I discussed with the Assistant Engineer that we would still comply with the Bylaw. To achieve this goal, our engineered plan (to be completed based on our final ARB Approved plan) will include a permeable paver system in the front patio area, a rain garden in the northeast corner and a Cultec tank system below the rear parking area. The front, right roof will be designated for a seasonal herb garden for the restaurant.

## 6. Parking Traffic

The information presented below was prepared by myself, based on my personal historical knowledge of the area and recent observations during various times throughout the day.

Currently there are three uses at 1211 Mass Ave.; a used car dealer, tire shop and 3-bedroom apartment. We have estimated that these uses generate 16 - 20 car trips, twice per day. The DAV, which is similar in size to the proposed restaurant area, was not included in the traffic analysis.

The mixed-use proposal is comprised of a fifty-room hotel and a restaurant containing approximately 2,500 square feet. Based on this use, we have estimated the car trips to the property, for hotel customers and all employees, to be approximately 18-25, twice per day. This assumes that forty percent of visitors will not be using automobiles. We also did not attempt to estimate the restaurant customer, as it is less than 3,000 square feet and is going to be equal in size to the DAV. This use will have the same impact as any other retail space on the Massachusetts Avenue corridor.

This property is located on Mass Ave. at the corner of Clark Street. As mentioned above, we anticipate 18-25 automobiles will be coming or going from the site on a very staggered basis, twice per day (excluding the restaurant).

The main route to enter and exit the site will be via the intersection of Clark Street and Mass Ave. For guests heading West, all traffic would turn right on Mass Ave. and continue or use Lowell Street. The guests using a car to go East will also use the same intersection, but take a left on Mass Ave. This intersection is approximately 150 yards West of the intersection of Appleton Street and Mass Ave. This provides sufficient space for customers to enter Mass Ave. well before this intersection. There are good sight lines in both the East and West directions.

The mixed-use proposal will have very little impact on the intersection of Appleton Street and Mass Ave. As mentioned earlier, the DAV space which was used much like a restaurant is being replaced with essentially the same use and amount of area. The projected net increase from the other uses is not a significant number of vehicles for a street such as Mass Ave.

The major “queuing” at this intersection takes place mostly driving West on Mass Ave. during the morning commute. We do not anticipate adding vehicles in this direction of traffic at that time. Approaching the intersection from the West, there is very rarely any above average backup.

The Ottoson Middle School is located at the corner of Quincy Street and Appleton Place. Access to the school is typically from this corner or side dead-end streets heading up Quincy Street to Gray Street

(including Leigh Street, Benjamin Road and Farmer Street). On the opposite side, it is accessed from Acton Street via Appleton Street. As noted above, very little, if any of our morning traffic would require using any of these streets to get to their destinations.

The parking is intended for the hotel. However; if available, some restaurant use may occur. As part of the TDM plan, we committed providing incentives to employees who do not commute by car. Below please find additional information regarding our commitment.

1 – We intend to charge an hourly/daily rate for parking on site. It would essentially be added to a guest bill which is typically done in many locations.

2 – For employees who commute together in a group of two or more, we will provide preferential parking accommodations.

3 – A subsidy of 50% of the cost for a monthly transit pass will be provided to all full-time employees (on a prorated basis for part-time help) who utilize public transportation on a daily basis.

4 – We have provided fourteen bicycle parking spaces for indoor (including some tools for repairs) and outdoor use. We are also working to have “lime bike” locate bikes in the area which we believe will meet the demand model they currently utilize. Again, with the proximity to the Minuteman bike path, we believe this will be very well received by visitors who would like to explore Lexington (and hopefully Arlington) via this unique route.

As part of our lease arrangements for the future tenants, all these provisions would be included in any agreements.



## Town of Arlington, Massachusetts

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### ARB Meeting Minutes

#### Summary:

8:30 p.m. -  
8:40 p.m.

- Board members will review and approve meeting minutes

#### ATTACHMENTS:

Type	File Name	Description
▢ Reference Material	Amended_Final_Minutes_ARB_4_24_19.pdf	ARB Amended Final Minutes 4/24/2019

**Arlington Redevelopment Board**

**April 24, 2019, 7:45 p.m.**

**Town Hall**

**PRESENT:** Andrew Bunnell (Chair), David Watson, ~~Eugene Benson~~, Kin Lau

**STAFF:** Jennifer Raitt, Director, Planning and Community Development, ~~Adam Chapdelaine~~, Town Manager

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The Board remained in session during Town Meeting. The Chair convened a meeting among ARB members to discuss Town Meeting business regarding the continued Town Meeting discussion of Article 16. Staff was present for the first portion of the meeting and was excused during deliberations and decision.

Members discussed that they listened intently to Town Meeting's concerns on Monday night and Town Meeting Members' desire for more information and involvement and additional discussion with members of Town Meeting. The ARB should therefore reconsider its position and choose to recommend a vote of No Action on articles 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, and 16. With a No Action vote, these articles will be referred back to the ARB and staff for further study and recommendation to a future Town Meeting.

The Board still believes these recommendations are a net positive for the town, and that the encouragement and growth of mixed-use and affordable housing development is critical to Arlington's progress and growth, but understand that Town Meeting would like more information.

The Board discussed the importance of delivering this message to Town Meeting prior at the opening of Article 16 tonight and that we encourage Town Meeting Members and their constituents to remain informed and to take part in the discussion and to fully participate as the conversation continues to final ZBL recommendations. It is also important to convey that this discussion is part of a broader conversation as a part of the ongoing master plan implementation process and that the board looks forward to a town-wide discussion.

| Lau moved to accept a vote of No Action, Watson seconded. All voted in favor (4~~3~~-0).

Adjourned to Town Meeting.



## Town of Arlington, Massachusetts

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### Correspondence received:

#### Summary:

Letter from Steve Revilak 6/27/2019  
LeRoyer Comments on Proposed Hotel

#### ATTACHMENTS:

	Type	File Name	Description
▢	Reference Material	Letter_from_Steve_Revilak_062719.pdf	Letter from Steve Revilak 062719
▢	Reference Material	LeRoyer_Comments_on_Proposed_Hotel.pdf	LeRoyer Comments on Proposed Hotel

111 Sunnyside Avenue  
Arlington, MA 02474  
June 27, 2019

**Arlington Redevelopment Board**

Arlington Town Hall  
730 Mass Ave.  
Arlington, MA 02476

Dear Arlington Redevelopment Board,

I'm writing to ask the Redevelopment Board to consider adding a "Change History" section to the town's Zoning Bylaw, to make it easier to determine how the bylaw changes over time, to see when specific sections were amended, and so fourth.

The change history could be placed in an appendix, which would be for informational purposes only, and not part of the Zoning Bylaw itself. The change history would list the town meeting and warrant article number when each zoning change was made, the sections affected, and a sentence or two about the nature of the change.

For concreteness, changes from Articles 17–19 might be notated as follows. Of course, this is only one possible representation.

Article	Sections	Description
4/2019 ATM, Art. 17	6.2	Complete recodification of sign bylaws
4/2019 ATM, Art. 18	5.7	Update to Floodplain District regulations, for consistency with Town Bylaws, Conservation Commission Regulations, and the Massachusetts Wetlands Protection Act
4/2019 ATM, Art. 19	5.8	Update to Inland Wetland regulations, for consistency with Town Bylaws, Conservation Commission Regulations, and the Massachusetts Wetlands Protection Act

Thanks for your time and consideration.

Sincerely,

Stephen A. Revilak



## Comments on Proposed Hotel/Restaurant at 1207-1211 Mass Ave, Arlington

To: Arlington Redevelopment Board and Jenny Raitt, ARB Secretary Ex Officio  
From: Ann LeRoy, 12 Peirce St., Arlington, 781-646-7254, [annleroy12@gmail.com](mailto:annleroy12@gmail.com), TMM Precinct 17  
Date: July 18, 2019

Thank you for providing the online materials for the upcoming ARB public hearing on the proposed hotel/restaurant complex at 1207-1211 Mass. Ave. This is an interesting idea and certainly offers some economic benefits to the community, but as a long-time resident of the immediate neighborhood I also have many reservations and questions.

I am offering these comments, and some factual corrections, prior to the meeting, and plan to raise specific questions after learning more from the presentations. I trust that some of the vague and incomplete aspects of the proposal will be addressed in future meetings and written communications, and I look forward to participating in this process.

My comments generally follow the outline in Jenny Raitt's memo of July 16, 2019:

### Special Permit Criteria

#### **Docket Summary and Special Permit 1., Section 3.3.3.A**

"... upon successful permitting, will combine the two properties for a unified mixed-use development."

#### **The description of the B2 zoning district in the Zoning Bylaw (page 5-24) states:**

B2: Neighborhood Business District. The Neighborhood Business District is intended for small retail and service establishments serving the needs of adjacent neighborhoods and oriented to pedestrian traffic, and mixed-use buildings. Locations are almost all along Massachusetts Avenue or Broadway. *The Town discourages uses that would detract from the district's small-scale business character or otherwise interfere with the intent of this Bylaw.*

How does this hotel proposal meet that description? It does not maintain the R1 and R2 single- and two-family character of the abutting neighborhood on Peirce St., Clark St., and Locke St. A hotel is not a demonstrated need of the neighborhood, though it may be so considered by the town in general. This project does not seem to be an example of "small-scale business character" compared to other businesses in the adjacent and nearby B2 districts. The B4 description seems to be assumed as the prevailing standard for the special permit, rather than the less-intrusive B2 description for this combined zoning district site.

This proposal does nothing to address the Town's often-stated and broadly demonstrated need for affordable housing. This is just the opposite and serves a completely different demographic.

Why are all permitting fees being waived? What is their dollar value? Is this a common practice for this type of project?

#### **SP2. Section 3.3.3.B.**

"The redevelopment of the DAV site and the adjacent outdated automotive use is desirable for the public convenience and welfare." See also Mr. Doherty's comment: "this proposed development will transform a blighted area..."

As a resident of this neighborhood block, I do not consider this area to be blighted or a public inconvenience. Several successful and long-established businesses serve the community, notably Precision Tire. It is unfortunate that the DAV building has been vacant for so long, and that the town has not maintained the landscaping in front, but I still take exception to this pejorative mischaracterization of the area as a rationale for redevelopment.

"A hotel and restaurant ... could be desirable to tour groups..."

Tour buses are never mentioned in either the proposal or in the ARB memo. How would they be dealt with? – see my related comments below about traffic and parking concerns.

**SP3. Section 3.3.3.C.**

"The application materials do not provide detailed information regarding the traffic impact of the new use."

This is an understatement, and thank you for raising many important questions that require additional information before any decision can be made on this project.

Clark Street a private way. What impact does that status have on this type of redevelopment, especially considering that Clark Street is proposed as the only source of ingress and egress, as you noted. How would Clark St. be adapted and improved (widened, regraded, etc.) to accommodate additional traffic? What about adding a sidewalk? There is currently only a short sidewalk on each side of the Mass Ave. end of the street. The suggestion of a "no right turn" or "left-turn only" sign for vehicles exiting the proposed parking area is a good one and should be examined to lessen negative impacts on the abutting residential streets.

Clark St. is already used as a regular cut-through street from Forest/Peirce St. to Mass Ave, especially during the morning and afternoon commutes and Ottoson Middle School drop-off periods, and during services and events at St. Athanasius Church. The entire Mass Ave. corridor from Forest Street to the Lowell Street merger and Dunkin Donuts presents dangerous traffic conditions for cars, bikes and pedestrians. Clark Street is very close to the Lowell Street merger/intersection, which is currently treated as a free-for-all, making access to Mass Ave. difficult in either direction.

All of the intersections and traffic patterns between Forest and Lowell St. should be studied carefully in a "trip generation analysis" and a complete "Transportation Demand Management Plan." Ideas for new painted crosswalks, walk lights, center islands to lessen the crossing distance, and/or other safety measures should be taken into consideration. The two bus stops near the Appleton intersection also need to be part of the studies. I would urge that the Transportation Advisory Committee (TAC) also be involved in this analysis since it affects a fairly long section of Mass Ave. and numerous intersecting streets.

In reference to Mr. Doherty's inadequate TDM Plan, the Minuteman Bikeway cannot be "accessed by a street directly adjacent to the property (Clark St.)." Clark St. dead-ends at Mill Brook on the other side of Peirce St. and there is no bikeway entrance off Forest St. Bicyclists would have to go around a couple of blocks to either Mill Lane or Ryder St. to access the bikeway. His comments about various ridesharing, valet, and shuttle options should also be analyzed more thoroughly, as noted.

As noted, the issue of tour buses or other shuttle vehicles has not been addresses, but it needs to be if that is an intended clientele for the hotel. Where would buses park for short stops and overnight? What size buses would be allowed on Clark Street and other side streets? I'm sure there are many other questions that experienced transportation experts would want to address as well.

**SP4. 3.3.3.D.**

"... the ARB will need more information regarding water and sewer usage."

Yes, indeed ! Mill Brook is only a short block away from the proposed parking area, and the land slopes down to the brook. Stormwater runoff is already a problem in the area. Does the Conservation Commission have any responsibility within this site and its surroundings?

**SP5. Section 3.3.3.E.**

"No special regulations are applicable..."

What does this mean? What possible special regulations could be applicable? I would like some explanation or referral to appropriate regulations to review.

**SP6. Section 3.3.3.F.**

The proposed mixed-use structure "will not impair the integrity or character of the district or adjoining districts ..."

This seems to be a subjective opinion by non-residents of the neighborhood. The prior discussion of traffic and parking suggests that this project could significantly change the integrity and character of the R1 and R2 neighborhood on nearby Clark, Peirce and Locke Streets. Within the R2 district there are numerous single-family homes and many families with young children.

While the addition of such a redevelopment could enhance the Arlington Heights connections to the rest of town via Mass Ave, please do not overlook that this is a quiet residential enclave already surrounded by varied business and industrial uses. Any major changes in activity, such as this project proposes, certainly will affect the adjoining districts !!!

**SP7. Section 3.3.3.G.**

"The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood."

I disagree. See my comments above.

## **Environmental Design Review Standards**

### **1. EDR-1 Preservation of Landscape**

I agree that there may be some open space benefits in this project compared to current conditions, and will look forward to seeing a complete landscape plan. I am especially concerned about the possible loss of some very large trees behind the DAV building.

### **2. EDR-2 Relation of the Building to the Environment**

I agree that the ARB should carefully examine all of the height and massing features and impacts of the proposed new building and the landscaping within the site.

### **3. EDR-3 Open Space**

See # EDR-1 above.

#### **4. EDR-4 Circulation**

I agree that much more information is needed about circulation on and around the project site, as outlined in Jenny's memo. Specific concerns that I share are regarding traffic impacts on Clark Street and Peirce Street, truck loading and unloading (is there a loading dock or such designated area?), time of day and length of delivery times, location and use of dumpster(s) and related waste management, and lighting impacts. I also reiterate my earlier concern about tour buses, shuttle buses, limousines, or other large capacity vehicles.

I have a question about the parking requirement table. The following is from the Zoning Bylaw, 6.1.4 Table of Off-Street Parking Regulations.

Hotel/motel	1 space per sleeping room, plus 1 space per 400 sq. ft. of public meeting area or restaurant space
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The requirement for 1 parking space per room is clear, and I don't have a problem with some reduction of that number. But why is there zero parking required in this case for the restaurant? The plans also show a lounge area in addition to the restaurant, so why is that space not included in the square footage requirement? The proposal does not indicate how many seats are accommodated in the restaurant or lounge, but presumably many users would not be guests of the hotel. Where are they supposed to park and why is that not addressed?

I see the asterisk "First 3,000 sf of non-residential space in mixed use projects is exempt" but I still wonder why the entire first floor of the proposed building is not included as "non-residential space." It must be more than 3,000 sf if the restaurant alone is 2,568 sf.

I agree, as noted for this entire section of EDR on circulation, that "The ARB should request additional information."

#### **5. EDR-5 Surface Water Drainage**

I agree that the applicant "must submit an engineered site plan showing surface water drainage systems and a stormwater management plan..." As noted above, this site slopes down into the back yards on Peirce St. and further toward Mill Brook. Several large trees are located behind the DAV building (northeast corner of the property), and any plan for removing them or other nearby trees must be documented, as well as a replacement and remediation plan. Their removal could affect erosion and drainage problems for the abutting houses.

#### **6. EDR-6 Utilities Service**

I agree that "the ARB will want to understand that the services carried on these poles will not be overloaded."

#### **7. EDR-7 Advertising Features**

I agree that any "condition of a decision by the ARB should include a requirement that the final signage be reviewed for compliance."

#### **8. EDR-8 Special Features**

I agree that many features noted in this section should be addressed with more detailed explanations and specific implementation measures. These concerns include noise-producing machinery, service and loading areas, air conditioning, truck idling, solid waste removal (dumpsters), etc.

#### **9. EDR-9 Safety**

I agree with the need to address questions raised about personal safety in and around the proposed building and parking areas, and about the location and types of lighting to ensure safety while not unduly intruding on neighboring homes on Clark and Peirce Streets.

## **10. EDR-10 Heritage**

While the current buildings on the proposed redevelopment site are not designated as historic, several significant buildings in the neighborhood are listed in the Arlington Historical Commission Inventory: 1210 Mass Ave (The Children's Room in an old Victorian house, 1880), 11-13 Lowell Street (the former Benjamin Locke Store from 1816, now residences), 23-25 Clark Street (two-family house, c. 1919), 11-13 Peirce St. (now a four-unit residence, Housing Corporation of Arlington, 1919) and several residential buildings from 1180 through 1218 Mass Ave, all within the Forest to Lowell St. corridor across Mass Ave from the proposed site. However, the current building at 1218-1222 Mass Ave. was built in 1986, replacing an historic house from 1898, so the inventory is incorrect and needs to be updated. Likewise, the current buildings at 1180-1184 are replacements that should be deleted from the AHC inventory.

The 11-13 Lowell and 11-13 Peirce Street buildings and 1180, 1188 and 1218 Mass Ave are also listed in the state MACRIS historic inventory. Again, 1180 and 1218 should be deleted since they are new structures. The Lowell Street property (1816) is also listed on the National Register of Historic Places. The historic Old Schwamb Mill is also in the immediate vicinity, at 17 Mill Lane, and is listed on all three inventories.

The recently completed Historic Preservation Survey Master Plan recommends that the following buildings on Mass Ave. be added to the AHC Inventory: 1189-1195 (the B2 commercial Locke Place). Most of the one- and two-family houses on Peirce, Clark and Locke Streets date from the early 1900s, although they have received no historic designations. They do, however, constitute a contiguous historic neighborhood.

## **11. EDR-11 Microclimate**

The proposed changes do not appear to significantly affect the immediate microclimate, however, the height of the new building may create shadows on adjacent residences, and the proximity of Mill Brook should raise questions about additional stormwater inputs and flooding impacts.

## **12. EDR-12 Sustainable Building and Site Design**

I agree that additional information is needed regarding the LEED checklist and related sustainability guidelines.

## **Conditions**

I appreciate the ARB's ongoing oversight of this proposed project as outlined in this section of Jenny's memo.

### **Hotel Management**

I do not know if the ARB, Select Board, Board of Health, or another entity has responsibility for this aspect of the project, but I would like to know more about the proposed hotel manager or a management company. Is the hotel to be operated by a local on-site manager, or will it be sold off to a national or international chain in the near future? What guarantees does the town have for management and oversight? Concerns have been raised about past abuses at Homewood Suites regarding prostitution and drug dealing, as well as other "non-traditional" uses of hotel rooms. Neighbors want assurances that any business that might be operated at this location will be safe, legal and appropriate to its surroundings in a residential part of town.